

CALMAT CAJON CREEK

SPECIFIC PLAN

SP 90-01

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INTRODUCTION

CALMAT CAJON CREEK SPECIFIC PLAN

I. INTRODUCTION

A. Project Overview

This Specific Plan encompasses approximately 1392 acres which will be utilized for industrial development, sand and gravel extraction and related uses, as well as open space. The CalMat Cajon Creek project is located partially within and adjacent to the northwesterly portion of the City of San Bernardino within the Cajon Creek Wash, just west of Cajon Boulevard, and approximately three-quarters of a mile south of the I-15/I-215 junction.

Implementation of this Specific Plan will require annexation of approximately 1184 acres to the City of San Bernardino, including about 180 acres owned by the County of San Bernardino. In addition, a Conditional Use Permit for Mining and associated Reclamation Plans will be needed for mineral resource extraction, as well as approval of Tentative Tract Maps for industrial development of portions of the property. A General Plan Amendment will also be required in order to bring certain aspects of the proposed industrial development, providing for industrial uses in areas now designated for extraction, into conformance with the City of San Bernardino General Plan.

B. Background

The CalMat Cajon Creek project and much of the surrounding area has been the subject of a number of planning activities which have had a role in shaping this Specific Plan. A concept plan for a project previously proposed by California Portland Cement Co. (a CalMat predecessor), known as the Tradelands, Inc. Cajon Creek project, was submitted to the City of San Bernardino in 1982. This project consisted of a major sand and gravel extraction and production operation over 816 acres of the present Specific Plan area. In addition to mineral extraction, approximately 70 acres were proposed for industrial development. A Memorandum of Understanding (Appendix K) was entered into between the City of San Bernardino and Tradelands, Inc. in July 1982, which provided for the establishment of these land uses, along with annexation of this area into the City. However, due to a pending merger of California Portland Cement Company into CalMat Co. which occurred in 1984, no further action on this project was taken.

Following the merger, the project was re-evaluated from an economic and land use standpoint. CalMat met with a number of public agencies with an interest in this area, including the Planning Departments from the City and County of San Bernardino, the City's Redevelopment Agency and the County Department of Transportation and Flood Control. CalMat also received a number of inquiries from parties interested in purchasing industrial property in the area, particularly rail served property. As a result, CalMat revised the project to include a balance of industrial uses (298 acres), mining-related uses (606 acres), and open space (488 acres).

C. Authority and Scope

The purpose of a Specific Plan, as defined by Title 7, Chapter 3, Article 8, Section 65450 et seq. of the California Government Code, is to carry out the systematic implementation of a General Plan. The CalMat Cajon Creek Specific Plan is consistent with Chapter 19.64 of the City of San Bernardino Development Code, pursuant to Government Code Section 65450 et seq.

The purpose of this Specific Plan is to review the project with respect to the following considerations:

1. Consistency with all existing State laws and local ordinances.
2. Consistency with the City of San Bernardino General Plan and Development Code.
3. Implementation of the plan would not be detrimental to the public interest, health, safety, convenience, or welfare of the City.
4. The subject property is physically suitable for the requested land use designations and the anticipated land use developments.
5. The proposed plan shall ensure development of desirable character which will be compatible with existing and proposed development in the surrounding neighborhood.
6. The proposed plan will contribute to a balance of land uses so that local residents may work and shop in the community in which they live.

7. Availability of public facilities and services adequate to serve the project.

Adoption of the CalMat Cajon Creek Specific Plan will include this text which establishes project conformance with existing land use plans and ordinances, along with a Land Use Plan which designates permitted land uses.

The Cajon Creek Specific Plan will be incorporated into the City of San Bernardino General Plan and Development Code by reference. As so adopted, the Specific Plan will govern the distribution, location, type, and extent of land uses in the Specific Plan area.

The Cajon Creek Specific Plan contains land use designations tailored closely to the topography, the vegetation, the natural resources, and the public infrastructure of the Specific Plan area. In this sense, it is a regulatory plan which will serve as the zoning ordinance for the property within its boundary. It is the intent of this Specific Plan that permitted land uses and types of development conform to the regulations specified herein, which may differ from the conventional, general regulations of the City of San Bernardino Zoning Ordinance and Development Code.

Because significant effort has been spent during the Specific Plan process in incorporating into the Specific Plan specific Planning Area Regulations and Design Guidelines for each Planning Area, approval of Development Permits will differ somewhat from the ordinance procedures set forth in Chapters 19.31 and 19.44 of the City's Development Code. Where specific development regulations are contained in the Specific Plan, those regulations shall apply. Where the Specific Plan is silent, the general regulations of the Development Code shall apply.

LOCATION AND SETTING

II. LOCATION AND SETTING

A. Project Location

CalMat's Cajon Creek 1392-acre project site, which includes approximately 180 acres of County ownership within the Cajon Creek Wash, is located partially within and adjacent to the extreme northwestern portion of the City of San Bernardino, approximately 5 1/2-miles northwest of the downtown (Figures 1 and 2). Approximately 208 acres of the southeastern portion of the CalMat property are currently within the City of San Bernardino. Although the remaining 1184 acres of the overall project site are located within the County of San Bernardino, the proposed project lies within the City's Sphere of Influence.

The location of CalMat's Cajon Creek ownership can be legally described as consisting of County of San Bernardino Assessors Parcel Numbers:

262-011-38	262-042-16
262-011-39	262-042-17
262-011-40	262-051-27
262-011-41	262-051-29
262-011-42	262-051-30
262-011-43	262-061-41
262-011-44	262-061-42
262-011-45	262-211-08
262-042-03	348-151-01
262-042-07	348-151-02
262-042-14	348-151-23
262-042-15	



SUB-REGIONAL LOCATION
FIGURE 2

RIVERSIDE 6

215

The proposed project also includes an additional approximately 180 acres owned by the County of San Bernardino located north of Institution Road (Figure 3) within the Cajon Creek floodplain, about 100 acres of which is proposed to be extracted under a lease agreement with the County. This County-owned land can legally be described as consisting of County of San Bernardino Assessors Parcel Numbers:

262-042-06	262-042-22
262-042-10	262-042-25
262-042-11	262-042-26
262-042-18	262-042-27
262-042-19	262-042-28
262-042-20	

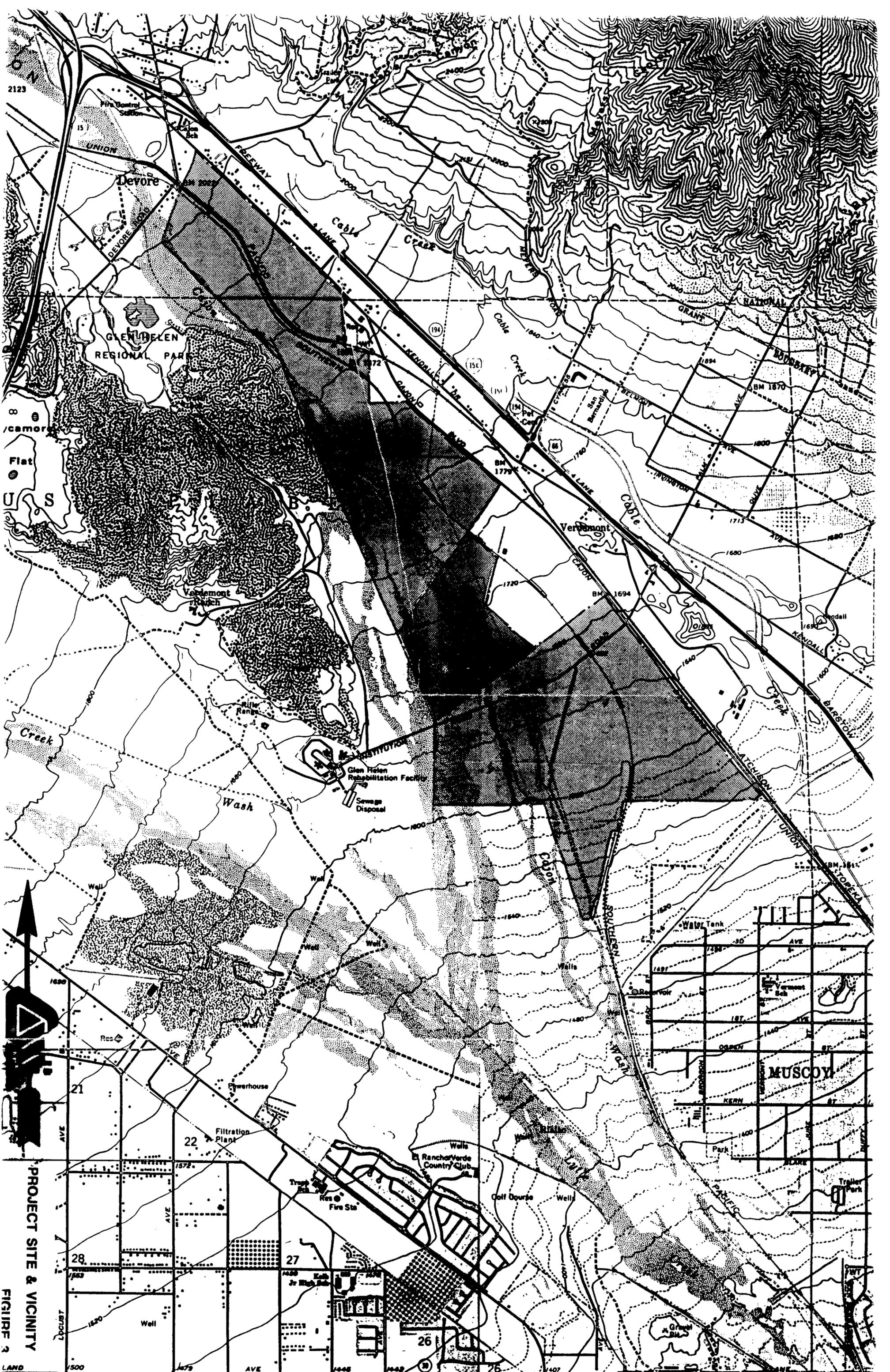
B. Site Access

Site access is available from I-215 (the Barstow Freeway), by means of the Palm Avenue and Devore Road freeway interchanges, and from Cajon Boulevard and Institution Road (Figure 3).

C. Site Characteristics

The CalMat Cajon Creek project site extends in a generally northwest/southeasterly direction along the west side of Cajon Boulevard for a distance of about four miles (Figure 3). It extends from just north of the Cable Creek channel (approximately 1.1 mile south of Palm Avenue/Institution Road) to about 1000 feet southeast of the Cajon Boulevard/Devore Road intersection. Actual CalMat ownership is divided into northerly and southerly portions, separated by a former County landfill (Figure 3). However, a portion of the project site within Cajon Creek Wash not owned by CalMat, located to the west of this landfill, is proposed to be leased from the County for extractive purposes. Thus, except for separations by railroad right-of-way ownership in several areas, the various portions of the project site can be considered to be contiguous. Portions of the westerly Cajon Boulevard frontage from north of the County landfill to just north of the Cajon Boulevard/Kendall Drive intersection are in private or City ownership.

Almost the entire site is comprised of gently-sloping terrain associated with the Cajon Creek Wash and its broad alluvial fan which extends from Cajon Canyon, located to the northeast of the I-15/I-215 junction, to just south of the CalMat ownership where Cajon Creek joins Lytle Creek.



2123

PROJECT SITE & VICINITY
FIGURE 2

LAND

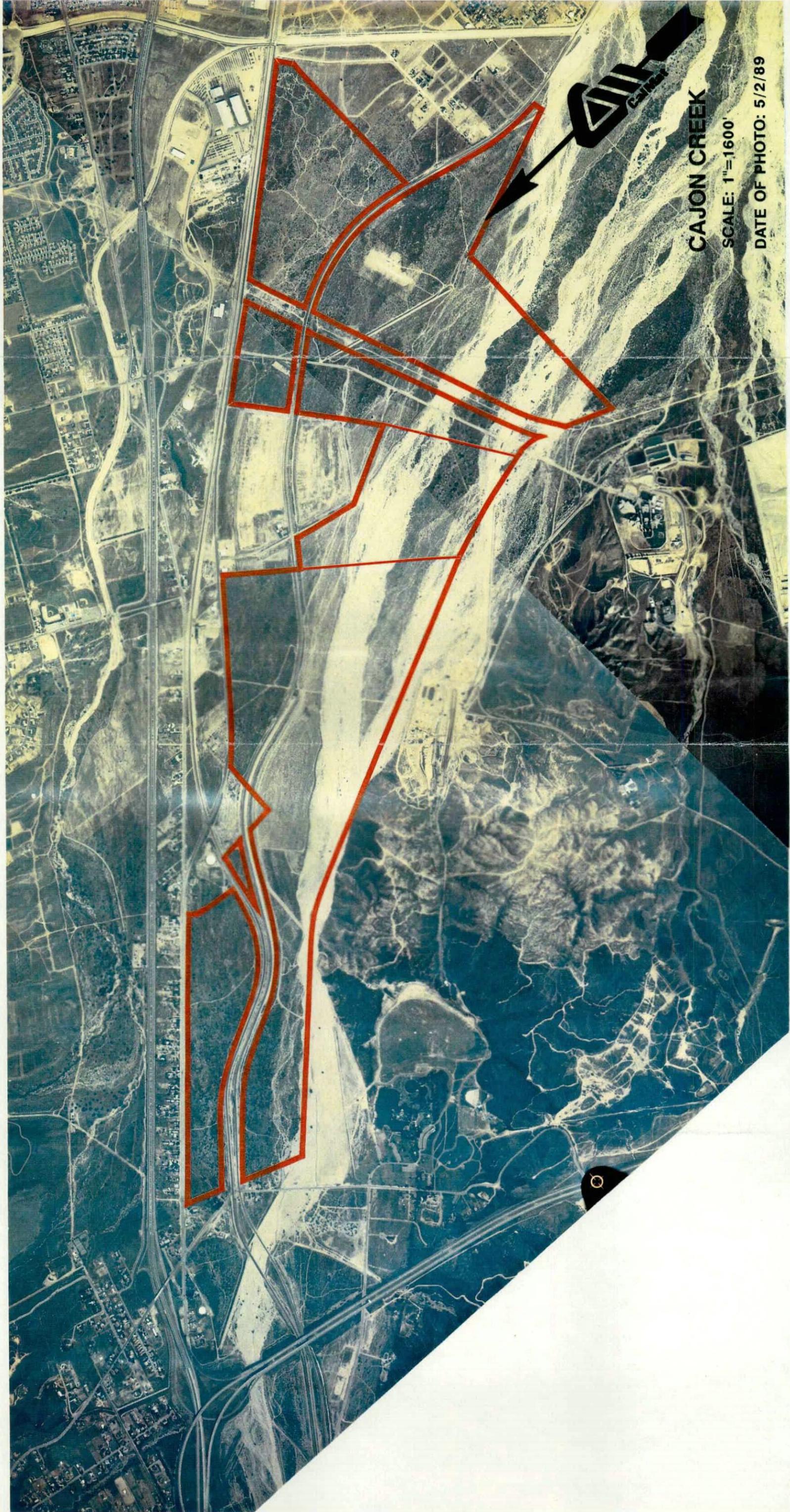
On-site elevations generally range from around 2020 feet MSL in the northern portion, to approximately 1680 feet just north of Institution Road, to about 1500 feet in the southern-most portion. The average north-south natural gradient is about 2.5 percent.

Three railroads, the Union Pacific, Southern Pacific and the AT&SF share a common main line road bed in the northerly portion of the project site (Figure 4). Approximately one mile south of Devore Road, the Southern Pacific diverges from the Union Pacific and AT&SF Railroads, which cross Cajon Boulevard about 2000 feet to the south and then generally parallel Cajon Boulevard's east side. The Southern Pacific continues southward on a raised road bed, approximately 20 feet above adjacent terrain, generally bisecting much of the Specific Plan area. It traverses Institution Road by means of a concrete overcrossing.

The 100-year FEMA floodplain of Cajon Creek Wash, on its east side, generally follows the Southern Pacific Railroad, jogging southwestward as a result of a groin designed to protect the westerly portion of the former landfill (Figure 3). It crosses Institution Road approximately one-third mile west of Cajon Boulevard, then follows Muscoy Groin No. 2 which extends generally southward from Institution Road for a distance of one-half mile, then jogs southeasterly across the southern part of the CalMat ownership, and again follows the Southern Pacific Railroad.

The westerly side of the Cajon Creek Wash 100-year FEMA floodplain southward from Devore Road, follows an approximately one-half mile long dike which protects Glen Helen Regional Park, and then follows the base of the hills to the south for a distance of about 0.8 mile, where it is again contained by a series of dikes which protect an adjacent off-road vehicle racetrack and the Glen Helen Rehabilitation Facility located to the southwest. Southward of Institution Road, the western side of the Cajon Creek floodplain converges with that of Lytle Creek. Thus, except for the planned 257-acre instream mining area, most of the CalMat ownership which lies to the east of Cajon Creek Wash is protected from 100 year floods by a combination of man-made levees and railroad embankments. A levee similarly protects the former County landfill.

Visually, the project site is generally a broad, flat, relatively featureless area, except for the raised Southern Pacific railroad main line which traverses the site. Most of the site contains scattered low-growing scrub-type vegetation with occasional scattered taller native shrubs. This Riversidean alluvial fan sage scrub, much of which is in a disturbed state, is typical of the overall Cajon Creek wash area. The CalMat Cajon



Creek site is almost completely vacant, except for an approximately 16-acre area within the south-central portion of the site which is on short-term lease to a model airplane club.

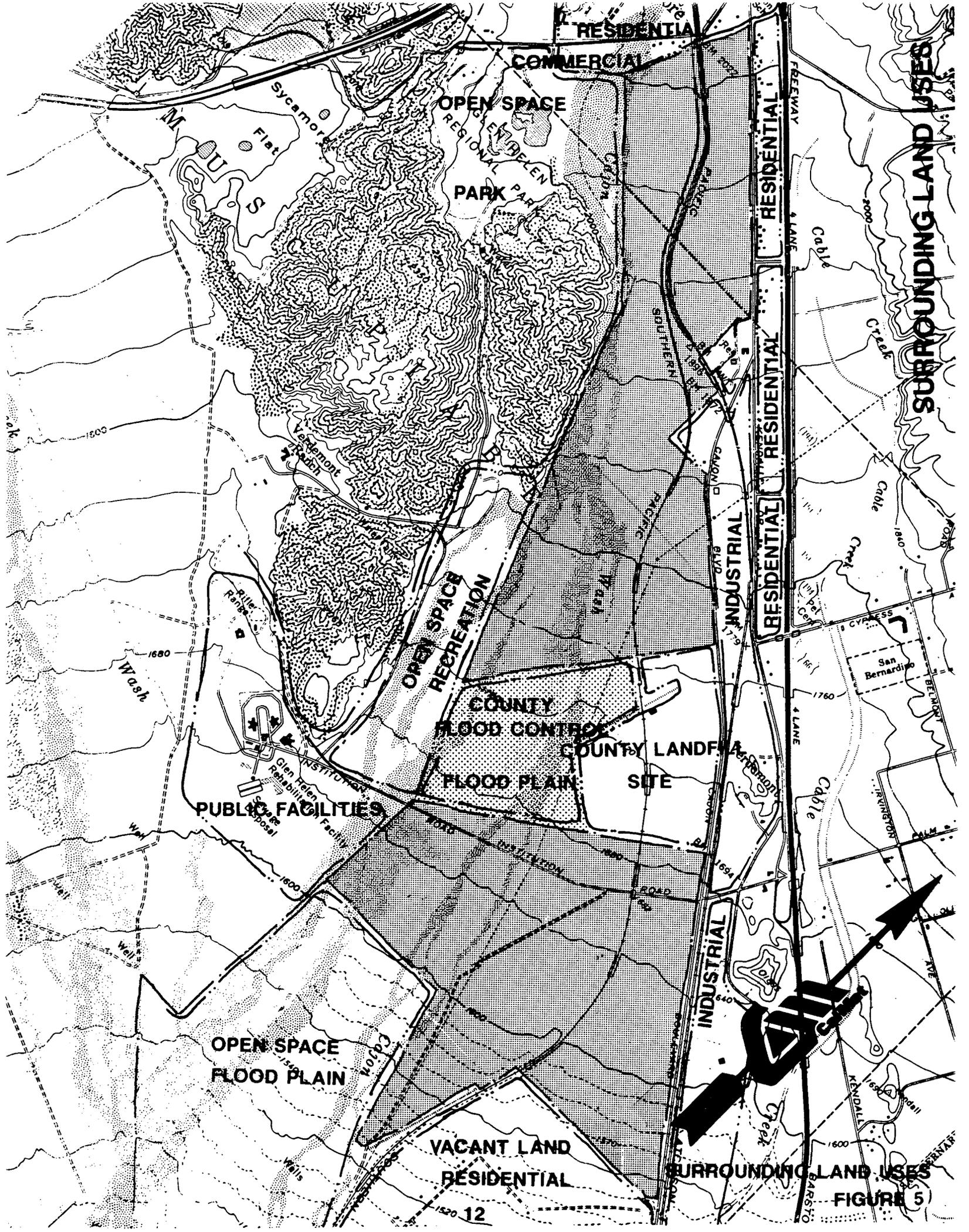
The Cajon Creek project site is traversed by overhead power lines of the Southern California Edison Company. These power lines pass through the eastern portion of the 100-year FEMA floodplain from the southern tip of the project site, cross the southern end of Muscoy Groin No. 2, pass over the western edge of the former County landfill, at which point they turn north-eastward, bisecting CalMat ownership, then cross Cajon Boulevard approximately 0.6 mile south of the Cajon Boulevard/Kendall Drive intersection. Smaller overhead power lines also cross the project site as they follow the north side of Institution Road, as well as parallel the CalMat ownership along the west side of Cajon Boulevard.

Two underground aqueducts cross the project site in an east-west direction. The Metropolitan Water District (MWD) line crosses Cajon Creek approximately 400 feet south of Institution Road, while the San Gabriel Valley Municipal Water District line parallels the MWD line about 100 feet further south. Two CalNeV petroleum pipelines traverse the Specific Plan area. A 14-inch pipeline follows the east side of the Southern Pacific Railroad, while an 8-inch pipeline traverses the west side of the Cajon Creek floodplain, adjacent to the CalMat property ownership. The location of these facilities can be seen in Figure 5.

D. Surrounding Land Use

Land uses surrounding the project site are widely varied in nature, including open space, recreation, industrial and residential (Figure 5). Portions of the site are somewhat isolated in character. This is primarily a result of the presence of the broad Cajon and Lytle Creek Washes and their floodplains; the upland area to the west generally associated with the Glen Helen Regional Park and Glen Helen Off Highway Vehicle Park; and the presence of three railroad main lines that pass through the area.

Land uses to the west of the CalMat Cajon Creek Specific Plan site, which are located to the north of Institution Road include Glen Helen Regional Park, reached by means of Devore Road; and the Glen Helen Off Road Vehicle Park, accessed by Institution Road and Ranch Road located to the west of Cajon Creek. These uses generally cover the upland area of rolling hills located between the Cajon Creek and Lytle Creek Washes. Located along



SURROUNDING LAND USES

SURROUNDING LAND USES

FIGURE 5

the southern portion of this upland area is the Glen Helen Rehabilitation Facility and an associated sewage treatment plant, and the Sheriff's Training Academy. The Verdemont County Boys Camp is located along the western side of this upland, to the west of the off-highway vehicle park.

Surrounding land uses to the west of the Specific Plan area, located to the south of Institution Road, are primarily vacant, broad floodplain areas associated with the confluence of the Cajon Creek and Lytle Creek Washes. A major sand and gravel extractive operation, Owl Rock Products, is located approximately one mile to the southwest of the southern aspect of the Specific Plan area in the Lytle Creek Wash. A 134-acre County of San Bernardino biological habitat preserve borders on about 1100 feet of the CalMat property on the west, immediately south of the MWD and SGMWD pipeline easements. This preserve contains Riversidean alluvial fan sage scrub habitat for two federally-endangered plants which require periodic natural flooding.

Land uses to the south of the project site include a continuation of the broad Lytle Creek Wash and approximately 15 scattered semi-rural homes within an approximately 235-acre area located south of the Specific Plan area, between the Cable Creek Channel and the railroad, which are accessed by 5th Avenue and a portion of Gray Street, both of which are unpaved. CalMat's Highland Avenue sand and gravel extractive and processing operation, and the Industrial Asphalt facility, are located two miles to the south, along the west side of Lytle Creek.

The unincorporated community of Muscoy is located south of the Cable Creek Channel, and between Lytle Creek and Cajon Boulevard, approximately one-half mile south of the Specific Plan area. A largely undeveloped area containing several houses extends westward of Cajon Boulevard along the north side of the Cable Creek Channel for a distance of about three-quarters of a mile. A school site has been proposed for this area by the San Bernardino Unified School District.

Surrounding land uses to the east of Cajon Boulevard and south of Palm Avenue include the Union Pacific and the AT&SF railroads, and heavy industrial uses. Land uses to the east of Cajon Boulevard between Kendall Drive and Palm Avenue consist primarily of scattered industrial uses, including the visually-prominent grain elevators of the Cargill Flour Milling facility, a woodworking facility and a gunite company. The San Bernardino Municipal Water District Palm Avenue Reservoir and Hydrogeneration Station is located to the north of Palm Avenue, between the railroad tracks and the freeway.

A number of adjacent land uses not associated with the project site, in addition to the former County landfill, are located to the west of Cajon Boulevard, between Institution Road and the Cajon Boulevard/Kendall Drive intersection. These include two small industrial buildings separated by Sheldon Avenue, a small street that joins Cajon Boulevard on the west just north of the landfill, as well as an automobile wrecking yard located just south of where the railroads cross Cajon Boulevard. Other adjacent uses in this area include a City of San Bernardino Municipal Water District water facility and a small motel just to the north, at the Cajon Boulevard/Kendall Drive intersection.

The east side of Cajon Boulevard, between the Cajon Boulevard/Kendall Drive intersection and Devore Road, contains an older residential area, whose homes front on Cajon Boulevard, and back onto the I-215 freeway. There are several mixed commercial uses within this area which is designated for future industrial land use on the City of San Bernardino General Plan.

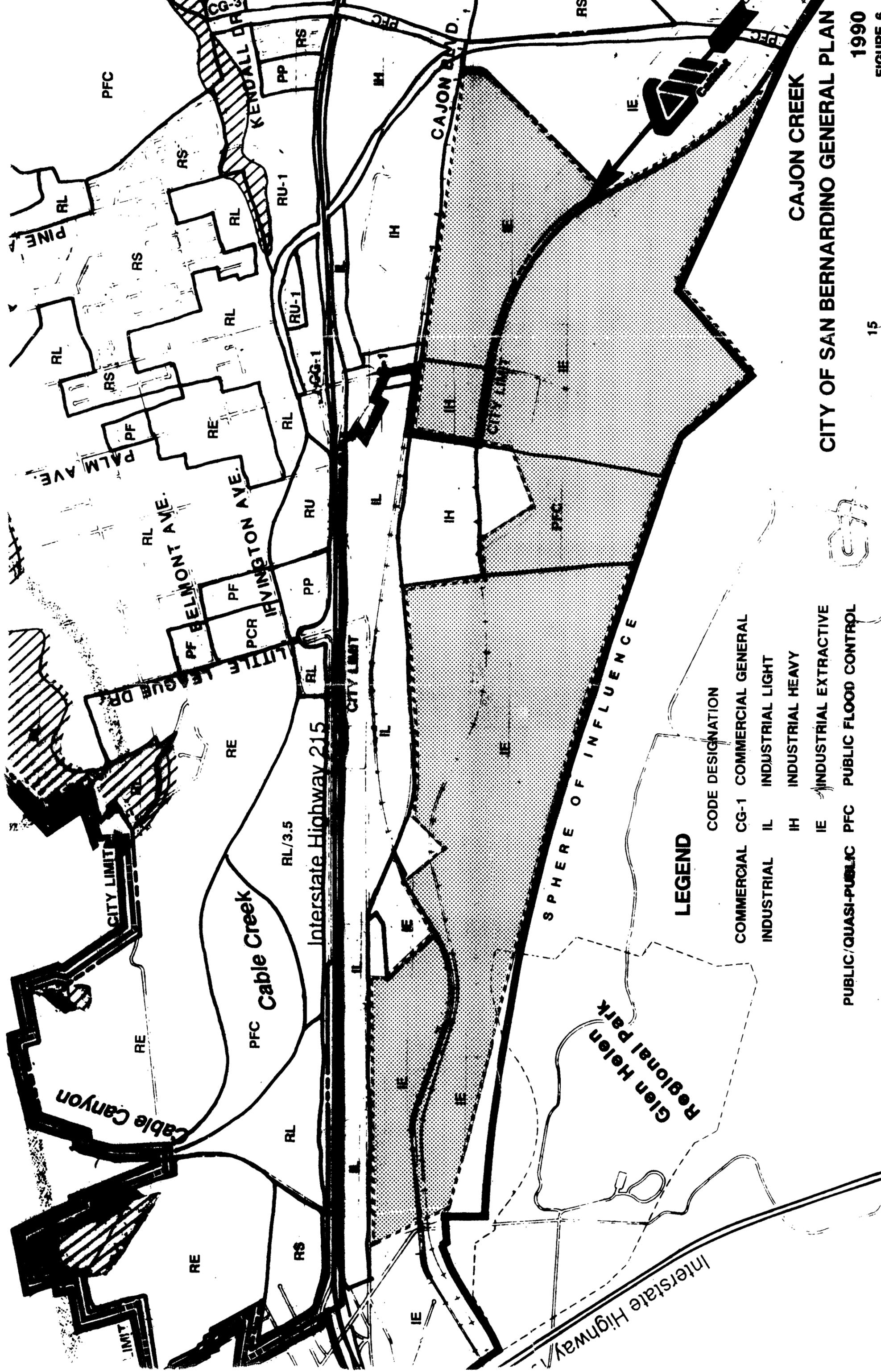
A small area along the west side of Cajon Boulevard, between Devore Road and the northern portion of the CalMat Cajon Creek project site, contains several homes and a City of San Bernardino water pumping station. An outdoor contractor storage yard is located to the north of Devore Road, between Cajon Boulevard and the railroad tracks, while land uses further north along Cajon Boulevard include a California Division of Forestry fire station and highway commercial uses.

E. Land Use Designation and Zoning

The City of San Bernardino uses a "one map" system of General Plan land use designation and zoning. That is, zoning is consistent with the land use shown on the General Plan. Existing land use designation and zoning for the Specific Plan area for both portions within the City and the City's Sphere of Influence include IE (Industrial Extractive), IH (Industrial Heavy), and PFC (Public Flood Control) (Figure 6).

The IH (Industrial Heavy) District is intended to provide for the continuation and development of heavy manufacturing industries in locations where they will be compatible with, and not adversely impact adjacent land uses.

The IE (Industrial Extractive) District is intended to promote the mining and processing of the City's mineral resources in the Cajon Creek, Lytle Creek, and Santa Ana River areas, while ensuring their compatibility with adjacent land uses. Additionally, this land use designation provides for the development of interim uses including, but not limited to, lumber yards, outdoor



LEGEND

CODE DESIGNATION

COMMERCIAL CG-1 COMMERCIAL GENERAL

INDUSTRIAL IL INDUSTRIAL LIGHT

IH INDUSTRIAL HEAVY

IE INDUSTRIAL EXTRACTIVE

PUBLIC/QUASI-PUBLIC PFC PUBLIC FLOOD CONTROL

CITY OF SAN BERNARDINO GENERAL PLAN

storage, plant nurseries, recreation (non-structural), etc., which do not impair the long-term ability to extract and process mineral resources.

The PFC (Public Flood Control District) is intended to provide for the continuation, maintenance and expansion of public flood control facilities.

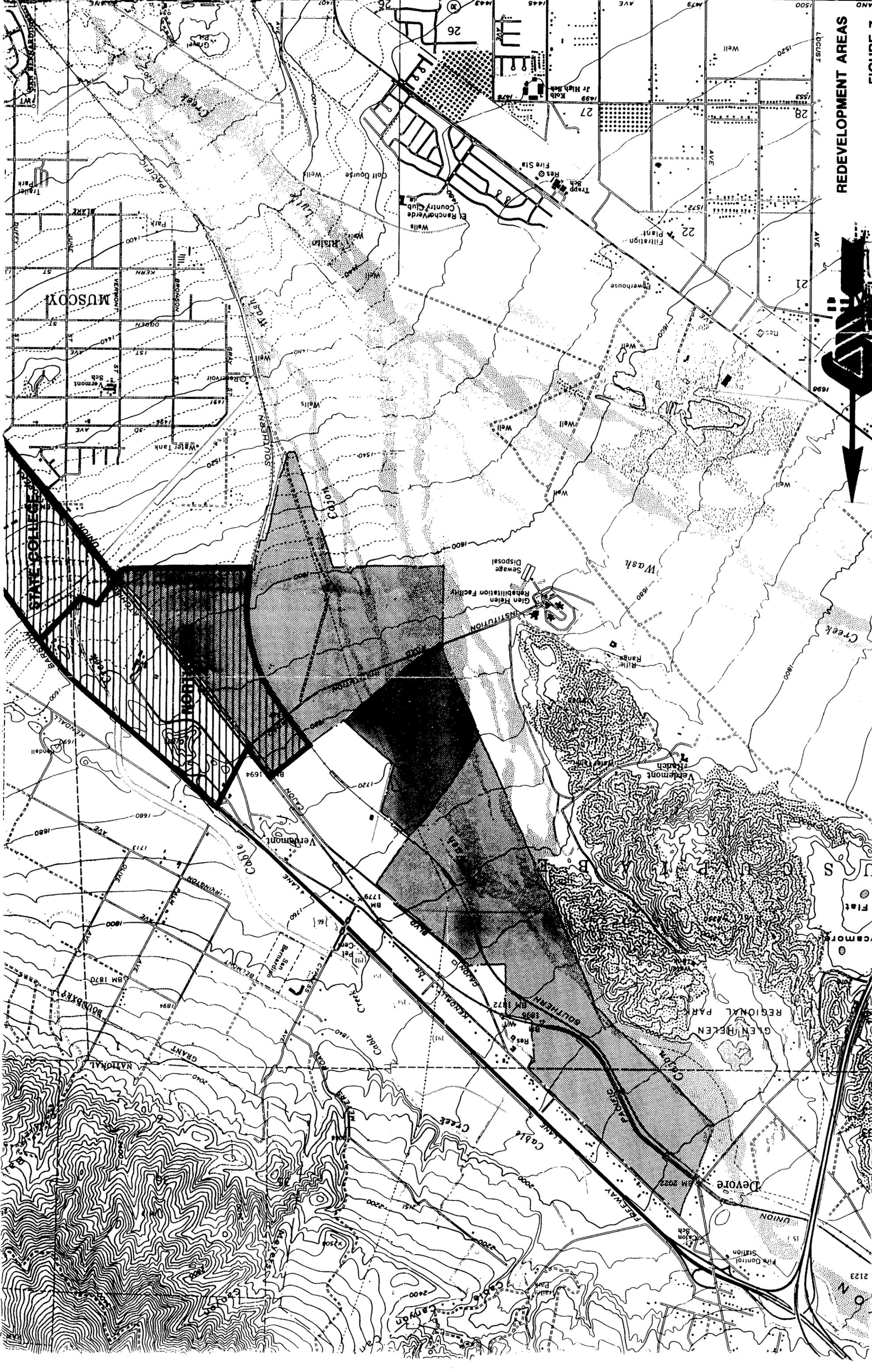
Allowable land uses in the unincorporated area located between the Specific Plan area and the I-215 freeway, as reflected on the City's General Plan, are Industrial Light (IL). The IL (Industrial Light) District is intended to retain, enhance, and intensify existing and provide for the new development of lighter industrial uses along major vehicular, rail, and air transportation routes serving the City. Land use to the immediate south of the Specific Plan area is Industrial Extractive.

This Specific Plan contains land use designations that are consistent with the City's General Plan. However, the Specific Plan contains land use designations that differ from the conventional, general regulations of the City of San Bernardino Development Code. It is the intent of this Specific Plan that permitted land uses and types of development conform to the regulations specified herein, which may differ somewhat from the general Development Code regulations.

F. Northwest Redevelopment Project

The Redevelopment Agency of the City of San Bernardino, organized in 1952 under the authority of the California Redevelopment law, is charged with the responsibility of revitalizing the City's deteriorating and distressed communities and neighborhoods. The Redevelopment Agency promotes the elimination of blight inside project areas; the stimulation of industrial, commercial, residential and public developments within project areas; an increase in local employment base; the enhancement of low and moderate income housing opportunities; an expansion in the regional boundaries of commercial activities; and an increase in tax increment in project areas.

The Northwest Redevelopment Project encompasses approximately 1500 acres, generally bounded by the I-215 freeway on the east, the Lytle Creek Wash on the west, Palm Avenue and the former County landfill on the north, and Seventh Street on the south. Approximately 215 acres of the Cajon Creek Specific Plan area are presently within the Northwest Redevelopment Project.



G. State Designation

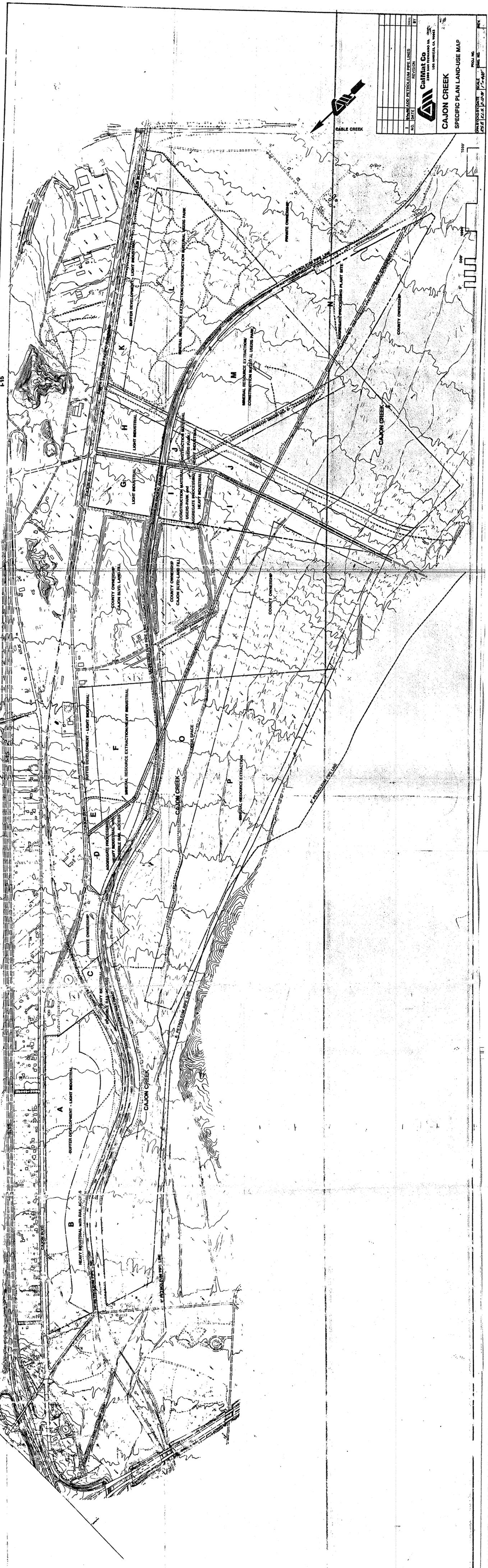
The California Department of Conservation, Division of Mines and Geology (CDMG), as part of its Special Report No. 143 (1987), estimated the total amount of sand and gravel that would be needed to serve the needs of the San Bernardino Region over a 50 year period, in accordance with the requirements of California's Surface Mining and Reclamation Act of 1975 (SMARA). CDMG geologists evaluated all major construction aggregate resource deposits within the region to develop a geological inventory of construction aggregate deposits. The Specific Plan area was classified by CDMG as containing significant sand and gravel resources (MRZ-2).

Because many of these potential sources were already committed to various urban uses which limit or preclude access to the underlying resources, a second step in the SMARA mineral land identification process was taken by the State. This resulted in the designation by regulation (by the State Mining and Geology Board) of those resources which are of regional significance. These are mineral deposits determined to be of prime importance in meeting the future needs of the San Bernardino Region, and which were felt to be available from a land use perspective. Almost all of the Cajon Creek project and adjacent areas to the north and south were so designated and identified by the State as Sectors C-3, C-5 and C-6 within SMARA Designation Report No. 5 (1987).

In June 1989, the City of San Bernardino adopted a new General Plan. The Mineral Resources section of the Natural Resources Element of this General Plan (Figure 8) identifies these Sectors (C-3, C-5 and C-6) and incorporates policies for their management. Specifically, these Sectors were zoned as IE (Industrial Extractive) on the Land Use Plan, and specific policies relating to sand and gravel extraction set forth. As required under General Plan Policy 10.7.7, this Specific Plan contains findings supporting the conversion of designated lands to other uses for Planning Area A and B (Appendix L), the only portion of the Specific Plan area designated that proposes uses other than mineral extraction-related or interim uses.

It should be noted that the City objected to the formal designation of portions of the "C" Sectors, (including several sites proposed for mining within the CalMat Cajon Creen Specific Plan site). Nevertheless, the majority of the mineral deposits within the Cajon Creek Wash area that were classified by CDMG as MRZ-2 were designated as regionally significant by the State. The General Plan identifies the State-designated Sectors, as well as those opposed by the City of San Bernardino (Figure 8).

General Plan Policy 10.8.3 requires the discretionary approval by the Planning Commission and Mayor and Common Council of specific development proposals for mineral extraction. The City's approval of the Specific Plan, accompanying Conditional Use Permits, and Environmental Impact Report satisfy the requirements of Policy 10.8.3 for the Specific Plan area.



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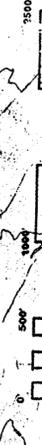


NO.	DATE	REVISION	BY
1		FOR PETROLEUM PIPE LINES	

CalMat Co.
 3800 SAN FRANCISCO BLVD.
 LOS ANGELES, CA 90031

CAJON CREEK
 SPECIFIC PLAN LAND-USE MAP

PROJ. NO. _____
 SCALE _____
 DATE 12.5.19.2019 17:44:07



**GENERAL PLAN
CONFORMANCE**

III. GENERAL PLAN CONFORMANCE

Goals, objectives and policies of the City of San Bernardino General Plan, as they relate to the Cajon Creek Specific Plan area and the nature of the development planned, are set forth in Appendix A. A concise statement of their overall intent is stated briefly below, followed by the manner in which the Specific Plan is in conformance with these goals, objectives and policies.

Land Use and Urban Design

Goal 1B Provide employment opportunities for existing and future residents of the City and those of adjacent communities. Provide for the development of 1,000 acres to accommodate new industrial uses.

Plan Conformance

The Specific Plan includes a total of 298 acres (which represents almost one-third of the General Plan goal) which will ultimately be developed with industrial land use. Of this, approximately 176 acres will be Light Industrial, while 122 acres will be Heavy Industrial. An additional 181 acres will be used as a Construction Materials Users Park, on an interim basis, for firms manufacturing products from aggregate produced within the Specific Plan area. Employment opportunities will also be provided by 606 acres of mining-related activities.

Goal 1D Provide for uses which capitalize on and respect the City's natural environmental resources, including mineral and agricultural production, and open space.

Plan Conformance

The Specific Plan provides for mineral resource extraction on 606 acres, of which, approximately 70 acres would be used as a permanent aggregate production plantsite. These areas have been designated as Regionally-Significant by the State Mining and Geology Board and recognized as such by the General Plan. Approximately 488 acres of the Specific Plan area have been set aside for open space uses, which may also be utilized for preservation and/or enhancement of biological habitats found within the Cajon Creek 100-year floodplain. Upon completion of in-stream mining and subsequent reclamation, an additional 257 acres will

be utilized for open space purposes. Development aspects of the Specific Plan have been designed to maintain the open space character of the broad Cajon Creek floodplain area.

Goal 1F Ensure that the types of land uses developed complement and do not adversely affect the quality of life and health of the City's residents and businesses. Control the development of industrial and other uses which use, store, produce or transport toxics, air emissions, and other pollutants.

Plan Conformance

The nature of the industrial development which will occur within the Specific Plan area will be governed by very specific Planning Area Regulations which provide more specificity as to the types of uses which will be permitted, as well as by development standards regarding setbacks, heights, lot coverage, etc. Each Planning Area within the Specific Plan also has site-specific Design Guidelines which development must follow, that will assure that significant land use impacts do not occur on neighboring properties.

Each Planning Area within which development will occur is also subject to the City of San Bernardino Development Code's Property Development Standards which prohibit the discharge of damaging emissions into the air or water, and require that emissions be in compliance with Air Quality Management District and Regional Water Quality Control Board regulations. These Property Development Standards also require that the use, handling, storage and transportation of hazardous substances comply with all applicable requirements of Government Code 65850.2 and Health and Safety Code 25505. In addition, all mineral resource extraction and processing will require approval of a Conditional Use Permit and Reclamation Plan, subject to environmental review which will, to the greatest extent possible, assure that such activities do not adversely affect the surrounding areas or the environment.

Goal 1H Continue existing and establish new residential, commercial, industrial, open space and public districts which are uniquely characterized by their functional role, permitted uses, density/intensity, and physical form.

Plan Conformance

The CalMat Cajon Creek Specific Plan, in conformance with the City of San Bernardino's General Plan, establishes this area as a major light and heavy industrial, mineral resource production

and open space area. These uses capitalize on this area's rail and freeway access that serve the Inland Empire, the presence of construction aggregate resources, and wildlife habitat within the Cajon Creek 100-year floodplain. No residential land use within the Specific Plan area has been provided for by the City of San Bernardino's General Plan.

In accordance with General Plan policies, the light industrial development within the CalMat Cajon Creek Specific Plan area will include manufacturing, warehousing, research and development, outdoor display and storage, and similar uses characterized by the location of their predominant activities in enclosed buildings, as appropriate to each individual Planning Area. In addition, in accordance with General Plan policies, light industrial Planning Areas permit supporting retail and personal service commercial uses, provided that they constitute no more than 15 percent of total building square footage. Retail sales of products manufactured on-site are permitted. A maximum floor area ratio of 0.75 for "employee-intensive" and 1.0 for warehousing and "hardware-intensive" uses is permitted. However a maximum height of two stories, but only 30 feet, rather than 50 feet is permitted unless it can be demonstrated that a greater height is appropriate. Design Guidelines and Development Standards of the Specific Plan require that buildings be designed to convey visual interest, and that they provide extensive landscaping and setbacks.

In accordance with General Plan policies, heavy industrial development will accommodate a diversity of heavy manufacturing uses appropriate to each respective Planning Area and its surrounding environs. The development of uses which contribute significant toxic wastes and pollution are prohibited, in accordance with Development Code Property Development Standards. Properties must be maintained free of debris and of an appropriate level of visual quality. A maximum floor area ratio of 0.75 with no defined height limit is permitted. Design and Development Guidelines require compatibility with adjacent land uses by means of landscaped setbacks, screening walls and other appropriate elements.

In accordance with General Plan Industrial Extractive policies, the Specific Plan provides for the utilization of a significant amount of on-site mineral resources that have been formally designated by the State, in such a manner as to be compatible with adjacent land uses. As appropriate, interim uses which do not impair the long-term ability to extract and process these mineral resources are permitted. All resource extraction and processing activities require approval by the City of a Conditional Use Permit which will review and apply appropriate

conditions which discourage truck-hauling trips through residential neighborhoods, mitigate noise, odor and dust impacts, and provide for visual integration with adjacent uses. Approval of a Reclamation Plan which provides for the reclamation of the site upon completion of extractive activities is also required. Both will require environmental approvals to assure mitigation measures are effectively carried out.

The Specific Plan retains 488 acres as open space within the broad Cajon Creek 100-year floodplain. Portions of this area may also be used for preservation and/or enhancement of biological habitats found in this area. Upon completion of mining and reclamation activities within Planning Area P, an additional 257 acres will be placed in open space, in accordance with Planning Area Regulations.

Goal 1L Achieve development which contributes to a high quality physical and visual environment. Ensure that development in the City's industrial districts is physically and visually reflective of their intended characteristics, and ensure that signage is well-integrated into architectural and site design.

Plan Conformance

The Planning Area Regulations and the Design Guidelines of the Specific Plan have been developed to assure that, on a site-specific Planning Area basis, high quality, visually attractive development appropriate to the type of land use results. They have also been designed to assure that these objectives will be met in comprehensive fashion for the Specific Plan area as a whole. Development within each Planning Area will require a comprehensive signage program.

Goal 1M Ensure that adequate public infrastructure is provided with land use development, and that phasing development is coordinated with the ability to provide adequate streets, sewers, water, electrical, natural gas, telecommunication, solid waste disposal, storm drainage, and other public infrastructure.

Plan Conformance

The Specific Plan contains an Infrastructure Improvement Plan (Appendix D) which specifies the various water, sewer, storm drainage and street improvements which will be required to serve proposed industrial development. In addition, the City Development Code's Subdivision Standards, as incorporated into the Specific Plan Planning Area Regulations, further assure that

basic infrastructure will be in place prior to actual development. The Environmental Impact Report (EIR) prepared for the Specific Plan and the mineral resource extraction Conditional Use Permit has confirmed the availability of other public and private services and facilities which will be required.

Goal 1N Maintain the integrity and health of the City's environmental resources and protect residents, businesses and visitors from the adverse impacts of environmental hazards. Protect significant environmental resources from disruption due to development. Protect life and property from the hazards of flood, fire, seismic risk and liquefaction.

Plan Conformance

In accordance with General Plan Environmental Resource Policies 10.1.1 - 10.3.4, a Biological Assessment (Appendix G) was conducted of the entire Specific Plan area. Appropriate mitigation measures have been developed to minimize development-related biological impacts and have been included in the EIR. All industrial development, and mineral resource processing plants, will be constructed outside of the FEMA-mapped Cajon Creek 100-year floodplain.

A Preliminary Geological Reconnaissance was conducted of the Specific Plan area in accordance with General Plan Geologic and Seismic Policies 12.1.1 - 12.4.4. Based on this study, mitigation measures have been set forth by the EIR which will reduce seismic risk to acceptable levels. The Specific Plan area is not located within an extreme or high fire hazard area, as set forth in the General Plan Natural Resources Element, but is located within a high wind area of the City. The EIR sets forth appropriate mitigation measures with regard to high wind protection.

Goal 4B Generate on-going industrial growth in an orderly and controlled manner through diversification of the industrial base and maintenance of current activity. Seek opportunities to provide industrial development requiring rail transportation.

Plan Conformance

The Specific Plan provides for a total of 298 acres of land to be developed in comprehensive fashion for industrial activity. Of this, approximately 47 acres is planned to have rail access, with the potential existing for rail to serve 24.5 additional acres. Given the availability of such large amounts of vacant industrial land, along with excellent location with respect

to the I-15 and I-215 freeways, the potential for diversification of the City's industrial base clearly exists.

Goal 5B Establish and maintain distinctive public spaces and rights-of-way which capitalize on existing physical assets that differentiate the City as a unique place in Southern California.

Plan Conformance

The Specific Plan area encompasses a total of 1392 acres which will be developed in comprehensive fashion in response to individual Planning Area Regulations and overall Specific Plan Design Guidelines. A total of 488 acres of the Cajon Creek 100-year floodplain, which is part of the extensive Cajon Creek/LytleCreek open space area, will remain undeveloped open space. Upon completion of in-stream mining, and subsequent reclamation, an additional 257 contiguous acres will be added to land which will remain undeveloped. Each Planning Area within the Specific Plan which will be industrially developed requires comprehensive landscaping, while the Design Guidelines portion of the Specific Plan sets forth very specific landscape guidelines, and requires that landscaping and open spaces be designed as an integral part of overall site plan design.

Goal 6A Achieve an integrated, balanced, safe and efficient transportation system that accommodates the demand for movement of people, goods and services. Provide for the design and construction of individual improvement projects that provide the capacity necessary to accommodate traffic forecast to be generated.

Plan Conformance

A traffic study (Appendix C) has been prepared to identify the nature of traffic which will be generated as a result of development which will occur within the Specific Plan area. This study also considered additional traffic which will impact the local circulation system as a result of other, non-project related development which will occur in the area so that the need for road improvements to accommodate both Specific Plan, plus all other future traffic could be identified. In conjunction with the City Department of Public Works, a determination was then made of necessary improvements to the existing street network, as well as the nature of new internal streets which will be required. The results of this study have been incorporated into the Infrastructure Improvement Plan (Appendix D).

Goal 6B Achieve a balance between parking supply and demand.

Plan Conformance

The Planning Area Regulations of the Specific Plan require that off-street parking be provided in accordance with Chapter 19.24 of the City of San Bernardino Development Code.

Goal 7A Establish and maintain adequate planning, construction, maintenance and funding for wastewater collection and treatment facilities to support land uses.

Plan Conformance

All light and heavy industrial development within the Specific Plan area will be served by public sewer. The Utilities Element of the General Plan requires that construction of facilities for connection to a public sewer be a condition of all discretionary projects in the City. It also requires that availability of adequate treatment capacity be a condition of project approval. Costs of infrastructure improvements may be borne by private development, the City, or a Redevelopment District, as determined appropriate at the time of development. Phasing of development will be coordinated with the Specific Plan Infrastructure Improvement Plan. Septic systems or other approved means of sewage disposal may be utilized by mineral resource extraction and processing facilities and interim land uses, in accordance with provisions of this Specific Plan.

Goal 7B Establish and maintain adequate planning, construction, maintenance and funding for water supply, transmission, distribution, storage and treatment facilities to support land uses.

Plan Conformance

All light and heavy industrial development within the Specific Plan area will be served by an adequate potable water supply, as required by the General Plan Utilities Element. Costs of infrastructure improvements may be borne by private development, the City, or a Redevelopment District, as determined appropriate at the time of development. Phasing of development will be coordinated with the Specific Plan Infrastructure Improvement Plan. An adequate supply of water for aggregate extraction and processing, as well as for allowable interim land uses, will be provided.

Goal 7C Establish and maintain adequate planning, construction, maintenance and funding for storm drain and flood control facilities to support land uses and preserve public safety.

Plan Conformance

All light and heavy industrial development within the Specific Plan area will be adequately served by storm drainage facilities. No permanent development will occur within the Cajon Creek FEMA 100-year floodplain. In-stream mining will occur within a portion of Cajon Creek located north of Institution Road. Phasing of storm drainage improvements will be in accordance with the Specific Plan Infrastructure Improvement Plan. As required by the General Plan Utilities Element, the San Bernardino County Flood Control District and the City Public Works Department shall review development plans to determine the need for storm drain facilities and flood protection, which will be a condition of project approval. Funding for storm drain facilities shall be provided through the plan check, permit, and inspection fees, the storm drain construction fund, and special assessments, or by the City or Redevelopment Agency, as determined appropriate at the time of development.

Goal 7D Establish and maintain adequate planning, maintenance and funding for solid waste disposal.

Plan Conformance

The EIR has confirmed the availability of adequate solid waste disposal facilities to serve Specific Plan development.

Goals 7E, 7F and 7G Provide adequate electricity, natural gas and telecommunications to serve new land use development.

Plan Conformance

The EIR has confirmed the ability of Southern California Edison, Southern California Gas Company, and General Telephone to provide necessary service. The General Plan Utilities Element states that new development within the City shall use available applicable funds to underground existing above-ground utility facilities.

Goal 8B Enhance the safety of residents through building and environmental design.

Plan Conformance

The General Plan Public Facilities and Services Element requires that the City's design review process ensure that all new projects have maximum visibility and security for entrances, pathways and corridors, open space and parking lots/structures. The EIR has confirmed the ability of the City of San Bernardino Police Department to provide adequate police protection.

Goal 8C Protect the residents of San Bernardino from the hazards of fire.

Plan Conformance

All buildings subject to City jurisdiction are required to adhere to fire safety codes. The EIR has confirmed the ability of the City of San Bernardino to provide adequate fire protection.

Goal 9A Improve the quality of life in San Bernardino by providing parks and recreation services to the City's residents. Establish an equestrian trail system along Cajon and Lytle Creeks.

Plan Conformance

CalMat will cooperate with the City in planning the location of an equestrian trail in Cajon Creek through the Specific Plan area. Such a trail must be compatible with planned development and in-stream mining (safety/liability), and must respect significant biological habitat areas. As no residential development will occur within the Specific Plan area, the need for parks is negated.

Goal 10A Conserve and enhance San Bernardino's biological resources, facilitating development in a manner which reflects the characteristics, sensitivities, and constraints of these resources.

Plan Conformance

A Biological Assessment was conducted of the entire Specific Plan area by a qualified biologist. No significant impacts are expected to result from development within the Specific Plan area as a result of project design and biological mitigation measures contained within the EIR. Although much of the area is within the Cajon Creek FEMA 100-year floodplain, no wetlands have been identified. The EIR has set forth mitigation measures and monitoring programs which will be required. Approximately 488 acres within the Cajon Creek 100-year floodplain will remain in open space in their natural state. In-stream mining

within a portion of Cajon Creek is not expected to significantly impact biological habitat in that area, some of which is highly disturbed. The Mining Plan will be designed to maintain the existing hydrologic regime by extracting to the creek's profile of equilibrium. As mining proceeds upstream, extracted areas will be recontoured and revegetated with Riversidean alluvial fan sage scrub species. Appropriate stabilization of the streambed will be provided to assure that mineral resource "skimming" operations will not significantly affect upstream or downstream habitat through excessive erosion or sedimentation.

Goals 10B, 10C and 10D Reduce incompatibilities between designated areas for mineral resource extraction and other land uses. Ensure adequate reclamation of mineral extraction areas.

Plan Conformance

The Specific Plan provides for extraction of on-site mineral resources formally designated by the State as regionally significant and reflected on the General Plan. These extraction sites have been identified based on land use, environmental and economic considerations.

Approximately 606 acres have been planned for resource extraction or permanent aggregate processing plantsites. Interim land uses, such as Construction Material Users Parks, have been planned for much of these areas prior to their extraction. An additional, approximately 36.5 acres will be used for aggregate processing on an interim basis. Approximately 50 acres of adjacent light industrial development has been incorporated into the Specific Plan in order to buffer permanently mining areas from Cajon Boulevard and land uses to the east. Thus, approximately 693 acres, or almost 80 percent, of the Specific Plan area designated by the State, and planned for extraction or development activity, would be utilized by, or be directly related to, mineral resource production.

Planning Area Regulations for each Specific Plan mining area set forth specific requirements designed to assure that resource extraction and processing will occur in an environmentally-sensitive manner and, to the greatest extent, be compatible with surrounding land use.

Approval of a Conditional Use Permit (CUP) by the City of San Bernardino will be required prior to the occurrence of any extraction or processing activity. The CUP will specify aggregate truck routes to be utilized to reach the freeway, which will minimize impacts on residential areas, and provide other operating restrictions or mitigating measures. Approval of a Reclamation Plan by the City will also be required in order to assure that

extracted land will be reclaimed for subsequent re-use. Both Conditional Use Permits and Reclamation Plans will be subject to environmental review.

Goal 10E Promote air quality that is compatible with the health, well being and enjoyment of life.

Plan Conformance

The General Plan Air Quality Element requires that, as a condition of approval of commercial and industrial projects, the City shall institute a Transportation Demand Management Program, in accordance with provisions of the Circulation Element. It also requires that the City, during the development review process, evaluate the location and air quality impacts of industrial uses, and require any necessary mitigation to ensure that they do not adversely affect adjacent residential uses. The air quality study, prepared as part of the EIR, indicates that air quality in the San Bernardino area already periodically exceeds South Coast AQMD standards. As a result, the EIR contains specific mitigation measures which must be carried out in order to assure that Specific Plan area air quality will not be significantly further impaired.

Mineral resource extraction and processing operations will require the issuance of Authorities to Construct and/or Permits to Operate from the South Coast AQMD in order to assure that such operations meet its stringent air quality requirements. In addition, the City's Development Code Property Development Standards, which will be adhered to by the Specific Plan, prohibit damaging fumes, vapor, gases or other forms of air pollution which could result from industrial development.

Goals 11A and 11B Conserve scarce energy and water resources. Manage and protect the quality of the City's surface waters and groundwater basins.

Plan Conformance

Solar energy design standards contained within the City's Development Code Property Development Standards are applicable to all Specific Plan development. Specific Plan Landscaping Design Guidelines specify that landscape design and construction should emphasize drought-tolerant landscaping, wherever possible, and require that all landscaping have an approved automatic irrigation system. Trees and shrubs must be planted so that, at maturity, they do not affect solar access. Specific Plan Architectural Design Guidelines specify that passive design concepts which make use of the natural climate to increase energy efficiency should be utilized. In addition, the Specific Plan states that water

utilized by permanent aggregate plant wet processing operations in Planning Area N, shall be clarified and re-circulated.

Development Code Property Development Standards, which have been incorporated by reference into the Specific Plan, require that no use which utilizes toxic substances or produces toxic wastes shall be permitted without approval of a Conditional Use Permit by the City. Such a use must have a toxic substance and waste management plan which provides for the safe use and disposal of these substances.

The Specific Plan Infrastructure Improvement Plan includes a storm drainage system to accommodate runoff from development areas. Planning Area Regulations for areas in which deep mining operations will be conducted, require that extractive operations be conducted in such a way so that all rainfall runoff is directed to drain internally into a pit or sump area, where it will desilt and percolate into the ground. Planning Area Regulations for Planning Area N, where a permanent aggregate processing plant site is planned, require that the Conditional Use Permit covering plant site operations shall provide for adequate surface drainage control so that excessive erosion or sedimentation problems do not occur in adjacent Cajon Creek.

Similarly, "skimming" extractive operations, planned to be conducted in Planning Area P must stabilize the downstream end, and measures must be taken to assure that flood flows maintain their pre-extraction velocities so that excessive downstream sedimentation does not occur. The Mining Plan will be designed to maintain the existing hydrologic regime by extracting to the creek's profile of equilibrium.

Goals 12A, 12B and 12C Substantially reduce the effects that would result from earthquake damage. Ensure the availability and effective response of emergency services, and prepare for the rapid, beneficial recovery from an earthquake.

Plan Conformance

A Preliminary Geologic Reconnaissance (Appendix E) was conducted of the area encompassed by the Specific Plan. This area is situated in a seismically active location near the junction of the San Andreas and San Jacinto Fault Zones. Potentially active faults related to the Glen Helen Fault have been mapped on site. The Glen Helen Fault has been included in an Alquist-Priolo Special Study Zone (APSSZ). The Specific Plan does not provide for the construction of any habitable structures in the APSSZ. However, given the complex tectonic setting of San Bernardino, other unmapped faults may be present. Liquefaction potential within the portions of the Specific Plan planned to be developed

is considered to be low because of the very coarse-grained deposits that predominate in the upper 50 feet, and given that groundwater levels are typically deeper than 50 feet below the ground surface. The EIR sets forth detailed mitigation measures that will be necessary prior to development occurring within the Specific Plan area.

Goals 13A and 13D Protect public health, safety and welfare, and the environment through proper planning for hazardous waste management. Minimize the threat of surface and subsurface water contamination and promote restoration of healthful groundwater resources.

Plan Conformance

All development which will occur within the Specific Plan area is subject to the City of San Bernardino Development Code's Property Development Standards which require that the use, handling, storage and transportation of hazardous substances comply with all applicable requirements of Government Code 65850.2 and Health and Safety Code 25505. The EIR sets forth specific mitigation measures which will be required to assure that significant adverse impacts from hazardous materials do not occur as a result of on-site industrial development, or mineral resource extraction or processing activity, and to protect surface water and groundwater from new contamination.

Goal 14A Ensure that City residents are protected from excessive noise.

Plan Conformance

The City of San Bernardino Development Code Property Development Standards require that exterior noise impacts do not exceed 65 dB(A) in residential areas. As a result, noise from on-site industrial or resource extraction activity must not exceed that level at the property line adjacent to any residential land uses. The EIR sets forth specific mitigation measures to assure that noise impacts in excess of City Standards do not result. In addition, Planning Area Regulations for Planning Area N, which is planned to contain a permanent aggregate processing plant, specifies measures to minimize the effects of aggregate truck traffic on residential areas to the south.

Goal 15A Protect people and property from the adverse impacts of winds.

Plan Conformance

The Specific Plan Architectural Design Guidelines require that, because of its high wind hazard location, all buildings be constructed and sited to withstand high wind hazards, and to prevent adverse funneling of wind on-site and on adjacent properties. Industrial buildings shall be designed to prevent wind funnel effects around their base and in passageways. In addition, the EIR specifies mitigation measures to help minimize the effects of periodic high winds on resource extraction areas and material stockpiles.

Goal 15B Protect people and property from brush fire hazards.

Plan Conformance

The Specific Plan Landscape Design Guidelines require that areas adjacent to open space portions of the Specific Plan within the Cajon Creek FEMA 100-year floodplain be designed and managed so as to minimize wildfire risks. In addition, development as proposed, will eliminate from concern a large area presently subject to brush fire danger.

Goal 16A Protect the lives and properties of residents and visitors to the City from flood hazards.

Plan Conformance

No permanent development will occur within portions of the Specific Plan area within the FEMA 100-year floodplain. The westerly portions of Planning Areas I and J, and the southerly portion of Planning Area N are, however, within the presently-mapped 100-year floodplain. A Hydrologic study (Appendix F) has indicated that Planning Areas I, J and N, as shown in Figure 9, are not within the actual 100-year floodplain. No development activity, as provided for by this Specific Plan, except for mineral resource-related haul roads, shall occur within the presently-mapped FEMA 100-year floodplain until the FEMA floodplain map has been officially revised. Prior to in-stream mining occurring within Planning Area P, Planning Area Regulations require that a "redline" engineering study be prepared by a qualified hydrologic engineer to determine the exact nature of "skimming" that may occur, so that excess resource material can be removed without impeding 100-year flood flow, and without causing significant upstream or downstream effects.

LAND USE PLAN

IV. LAND USE PLAN

A. Development Concept

The CalMat Cajon Creek Specific Plan provides for a mix of industrial, mineral resource extraction, and open space uses. Industrial development will occur on a total of 298 acres, of which approximately 47 acres will be oriented toward rail-served manufacturing and distribution uses, with another 24.5 acres having rail potential. Mineral resource extraction will occur, over the long term, within three areas totalling 606 acres. Approximately 181 acres will be used as a Construction Material Users Park (CMUP), on an interim basis. The duration of interim uses, such as CMUP, aggregate processing, etc., will be determined by resource depletion and market demand factors. An additional 88.5 acres, plus a portion of a 36.5 acre area, will be used by the aggregate processing plants and related facilities. The remaining approximately 488 acres, within the Cajon Creek 100-year FEMA floodplain, will be preserved as natural open space. This Specific Plan has endeavored to remain consistent, to the greatest extent possible, with the City of San Bernardino's Industrial Extractive (IE) land use designation and zoning, while respecting adjacent land use and the environmental concerns.

The Specific Plan area, within a regional context, is strategically located, as it is central to the Inland Empire, the Victor Valley area and the Ontario Airport, and it is reasonably close to the East San Gabriel Valley market. In addition, the market analysis (Appendix A) has indicated that the relatively affordable land and building pricing of this area is expected to stimulate demand for industrial land in the foreseeable future. In a general sense, the Specific Plan is designed with built-in flexibility in terms of land uses and the ability to upgrade to higher uses as the overall development of the project matures.

The Plan provides for light industrial development to serve as a physical and visual "buffer" along Cajon Boulevard. This will screen the more varied industrial uses and mineral resource activities that will occur behind and to the west.

Light industrial development is envisioned to be free-standing buildings of 10,000 to 40,000 s.f., occupied primarily by firms actively involved in light manufacturing and assembly, and compatible uses. Because of its location adjacent to the Southern Pacific, AT&SF and Union Pacific Railroads, portions of the northern part of the Specific Plan area will be developed with both rail-served heavy manufacturing (30,000 to 80,000 s.f. buildings) and warehousing and distribution operations (80,000 to

150,000 s.f. buildings). The Construction Materials Users Park interim use of portions of the Specific Plan area will include a variety of businesses tied to the construction industry in the Inland Empire. The primary attraction for these users will be the ready and convenient access to raw materials (sand, rock and gravel) mined from the Specific Plan area by CalMat for manufacturing concrete and stone products.

The areas proposed for mineral resource extraction have been formally designated by the State as being regionally-significant and of prime importance in meeting the future needs of the San Bernardino region. The Natural Resources Section of the City of San Bernardino General Plan recognizes these areas as being of regional significance, while the General Plan's Land Use Element designates these areas as Industrial Extractive. All areas which will be mined will be reclaimed in accordance with the provisions of the California Surface Mining and Reclamation Act of 1975, as amended, and the City of San Bernardino Development Code.

B. Specific Plan General Land Use Categories

The allocation of land uses within the CalMat Cajon Creek Specific Plan is the result of a number of decisions based on planning and marketing efforts directed at identifying opportunities and constraints within the Specific Plan area. Market feasibility (Appendix A) and infrastructure studies resulting in the Infrastructure Improvement Plan (Appendix D) have been conducted to determine the optimal mix of land uses.

An accompanying General Plan Amendment (GPA 91-18), provides for a number of land use designation changes, to both interim and final Specific Plan land uses. As can be seen in Figure 8A, approximately 150 acres of IE (Industrial Extractive), representing Planning Areas A, E, H and K, will become SPIE (Specific Plan Industrial Light) as final land uses. Twenty-six acres of Planning Area G, presently designated IH (Industrial Heavy), will become SPIL. A total of 67 acres of Planning Areas B, C and J, presently designated IE, will become SPIH (Specific Plan Industrial Heavy). An additional 334 acres of presently designated IE will become SPIE as an interim use, subsequently becoming SPIH as its final land use.

Approximately 384 acres of present IE will become SPOS (Specific Plan Open Space) as a final use. Another 168.5 acres of existing IE will become SPIE as an interim use, and subsequently become SPOS as a final use. Approximately 103 acres of the County of San Bernardino lease area presently designated PFC (Public Flood Control) will become SPIE as an interim use, and later revert back to its PFC designation. The remaining portions of the Specific Plan area will remain unchanged from their present General Plan land use designations.

The following is a description of the general land use designations of this Specific Plan. Specific permitted uses are designated within the Planning Area Regulations in Section V (Implementation).

1. SPIE (Specific Plan Light Industrial)

This designation is intended to permit manufacturing, warehousing, distribution, research and development, service uses, mini-storage, and similar uses characterized by the location of their predominant activities in enclosed buildings. Supporting retail and personal service commercial uses are permitted.

2. SPIH (Specific Plan Heavy Industrial)

This designation is intended to provide for the development of heavy manufacturing uses in locations where they will be compatible with and not adversely affect adjacent land uses. Where appropriate, these areas may be rail-served. Permitted activities accommodate a diversity of heavy manufacturing uses which are often characterized by large land utilization and physical facility requirements, and low employment intensity. The development of uses which contribute significant toxic wastes and pollution are prohibited.

3. SPIE (Specific Plan Extractive Industrial)

This designation is intended to accommodate sand and gravel resource production and related processing operations in a manner compatible with adjacent uses. Appropriate conditions are to be applied to ensure that mineral resource production activities which:

- a. preclude truck hauling through residential neighborhoods, unless there are no feasible alternatives,
- b. mitigate noise, odor and dust impacts,
- c. provide for visual integration with adjacent uses through the use of effective buffering techniques,
- d. provide for the restoration and reuse of the site upon completion of production activities, in a manner which does not adversely disrupt the character and integrity of adjacent uses.

A Reclamation Plan, in accordance with the California Surface Mining and Reclamation Act of 1975 and the City of San Bernardino Development Code, is required.

The development of interim uses which do not impair the long-term ability to extract and process the mineral resources, such as construction materials users activities, outdoor storage, concrete product manufacture, landscape nurseries and similar uses are permitted.

4. SPOS (Specific Plan Open Space)

This designation applies to land that is within the 100-year floodplain of Cajon Creek, as mapped by FEMA. Land within this designation may also be used for natural habitat preservation and restoration. Mineral resource extraction and limited primary processing may also be permitted, providing appropriate operational conditions are applied. A Mining and Reclamation Plan, in accordance with the California Surface Mining and Reclamation Act of 1975 and the City of San Bernardino Development Code, as well as permits from the U.S. Army Corps of Engineers and the California Department of Fish and Game, if applicable, are required.

5. PFC (Public Flood Control)

This designation is intended to allow for the continuation, maintenance and expansion of public flood control facilities.

A Reclamation Plan, in accordance with the California Surface Mining and Reclamation Act of 1975 and the City of San Bernardino Development Code, is required.

C. Allowable Land Use Types

The following defines allowable land uses within specified Planning Areas of the Specific Plan. These land use types differ somewhat from the land use types set forth in the City of San Bernardino Development Code. In determining permitted uses in the Specific Plan area, these definitions, rather than the Development Code definitions of land use types, shall control.

1. Automotive and Equipment: Fleet Storage -- This use type refers to the storage or parking of one or more vehicles used regularly in business operations. Excluded from this use type are Automotive and Equipment: Sales/Rentals (all types), and the incidental parking of vehicles as an accessory use to a permitted use on the same premises. Typical uses include mobile catering truck storage or delivery truck fleets.

2. Automotive and Equipment: Heavy Equipment Repairs -- This use type refers to the repair of motor vehicles such as aircraft, boats, recreational vehicles, trucks, etc., as well as the sale, installation and servicing of automotive equipment and parts, together with body repairs, painting and steam cleaning. Typical uses include truck transmission shops, body shops or motor freight maintenance facilities.

3. Automotive and Equipment: Light Equipment Repairs -- This use type refers to the repair of automobiles and the sale, installation and servicing of automobile equipment and parts, but excluding body repairs and painting. Typical uses include muffler shops, auto repair garages or auto glass shops.

4. Automotive and Equipment: Heavy Equipment Sales/Rentals -- This use type refers to the sale, retail or wholesale, and/or rental from the premises of heavy construction equipment, trucks and aircraft, together with incidental maintenance. Typical uses include aircraft dealers, boat dealers, or heavy construction equipment dealers.

5. Automotive and Equipment: Farm Equipment Sales/Rentals -- This use type refers to the sale, retail or wholesale, and/or rental from the premises of farm equipment, together with incidental maintenance. Typical uses include farm equipment dealers.

6. Automotive and Equipment: Non-Operating Vehicle Storage -- This use type refers to the storage of non-operating motor vehicles. Typical uses include storage of private vehicles, towaways or impound yards.

7. Automotive and Equipment: RV and Boat Storage -- This use type refers to the storage of Recreational Vehicles (RV's) and boats. Typical uses include the collective storage of personal RV's or boats.

8. Building Maintenance Services -- This use type refers to establishments primarily engaged in the provision of maintenance and custodial services to firms, rather than individuals. Typical uses include janitorial, landscape maintenance, or window cleaning services.

9. Business Equipment Sales and Services -- This use type refers to establishments or places of business primarily engaged in the sale, rental or repair of equipment and supplies used by office, professional and service establishments to the firms themselves, rather than to individuals, but excludes automotive, construction and farm equipment. Typical uses include office equipment and supply firms, small business machine repair shops, or hotel equipment and supply firms.

10. Construction Material Users Activities -- This use type refers to establishments that generally require significant amounts of construction-quality aggregate for the manufacture of their products, and can greatly benefit from a nearby source of supply. By their nature, they require large areas for open storage and curing of their products, and their facilities can be interim in nature and readily removable for subsequent re-use of the land. Such uses are compatible with adjacent mineral resource extraction and processing, and related activities. Typical uses include the manufacturing of concrete blocks, concrete pipe, and roofing material.

11. Construction Sales and Services -- This use type refers to establishments or places of business primarily engaged in construction activities and incidental storage on lots, other than construction sites, as well as the retail or wholesale sale, from the premises, of materials used in the construction of buildings or other structures other than retail sale of paint, fixtures and hardware; but excludes those classified as one of the Automotive and Heavy Equipment use types. Typical uses include building materials stores, tool and equipment rental or sales, or building contractors.

12. Contractor Storage Yards -- This use type refers to the outdoor storage of vehicles, equipment and incidental storage of materials used at construction sites other than the premises. Such uses must be conducted entirely within a building or within an open space entirely enclosed by a six-foot high fence or masonry wall. Typical uses include grading or pipeline construction contractor storage yards.

13. Gasoline Sales -- This use type refers to establishments or places of business primarily engaged in the retail sales, from the premises, of petroleum products, and includes the sale of tires, batteries, and replacement items, lubricating services and minor repair services. Car washes may be provided as an ancillary on-site use. Typical uses include automobile service stations, filling stations or truck stops.

14. General/Heavy Industrial -- This use type refers to industrial establishments primarily engaged in heavy manufacturing, compounding, processing, assembling, packaging, treatment or fabrication of materials and products. Such uses are generally characterized by their large land utilization and physical facility requirements. Open air storage, distribution and handling of materials and equipment is permitted. Uses prohibited include: the curing and tanning of raw hides and skins; the manufacture or storage of fireworks, gunpowder or explosives; the reduction of garbage, dead animals or refuse; stockyards or the slaughter of animals; smelters or smeltery and other uses which contribute significant toxic wastes and pollution. Typical uses include: woodworking mills, metal working shops, food processing, soft drink bottling plants, lumber distribution centers and grain elevators.

15. Manufacturing/Light Industrial -- This use type refers to a wide range of establishments engaged primarily in the design, development, manufacturing, fabricating and/or assembly of manufactured products which do not produce undesirable noise, vibration, odor, dust, smoke, glare or other nuisances. Storage or packaging of products is permitted only when a minimum of 50 percent of the gross floor area is devoted to the manufacturing process of any such products. Typical uses include electronic assembly firms, publishing and printing establishments and pharmaceutical manufacturing.

It is intended that office buildings be prohibited. However, regional and headquarter offices of business, industry and governmental agencies are permitted, when

such use is restricted to one single company or agency per parcel of land.

16. Mineral Resource Extraction and Processing and Related Activities -- This type refers to the mining and processing of mineral resources, including rock, sand and gravel. Such uses include:

- a) Extraction, processing, weighing, storage and loading of rock, sand and gravel (aggregate), clay and soils;
- b) Manufacturing, producing, processing and storing of asphaltic concrete, portland cement concrete, concrete products, clay products, top soil (using imported amendments), cement-treated base, and the recycling (re-crushing) of imported asphaltic and portland cement concrete;
- c) Sale and distribution by truck, rail, or conveyance of those items listed above;
- d) Off-street parking and fleet maintenance;
- e) Installation, construction, maintenance and replacement of structures, machinery, equipment and facilities incidental to the uses described above;
- f) Accommodations for on-site caretaker/security personnel;
- g) Other uses as may be determined incidental or accessory and approved by the Planning Director.

Typical uses include sand and gravel extraction and processing operations, ready-mixed concrete and asphaltic concrete plants.

17. Open Space -- This type of use is often applied to hazard or natural resource areas. In the case of the Cajon Creek Specific Plan, open space relates primarily to flood control, biological resources and in-stream mining of regionally-significant mineral resources. No permanent structures, other than for

flood control, streambed stabilization or haul roads are permitted, nor are activities which would be incompatible with in-stream mining by creating unnecessary safety or liability concerns. Typical uses include in-stream mining and haul roads, flood control structures, hiking and equestrian trails, and areas of biological habitat enhancement or preservation.

18. Recycling Collection Centers -- This type of use refers to establishments or places primarily engaged in collecting recyclable household materials for the purpose of resource recovery. Typical uses include collection centers that use portable containers for collecting household containers, metal containers, glass containers and paper or plastic.

19. Repair Services, Consumer -- This use type refers to establishments primarily engaged in the provision of repair services to individuals and households rather than firms, but excluding Automotive and Equipment use types. Typical uses include appliance repair shops and manufacturers' service centers.

20. Research and Development (R&D) -- This use type refers to establishments primarily engaged in research of an industrial or scientific nature which is provided as a service or which is conducted by and for a private firm, but excludes medical testing and analysis and product testing. Typical uses include electronics research laboratories, space research and development firms or pharmaceutical research labs.

21. Wholesaling, Storage and Distribution: Mini-Warehouses -- This use type refers to storage or warehousing service within a building(s) primarily for individuals to store personal effects, and by businesses to store material for operation of an industrial or commercial enterprise located elsewhere. Incidental uses in a mini-warehouse may include the repair and maintenance of stored materials by the tenant, but in no case may storage in a mini-warehouse facility function as an independent retail, wholesale, business or service use. Individual storage spaces within a mini-warehouse shall have a maximum gross floor area of 400 square feet and shall not be used for workshops, hobbyshops, manufacturing or similar uses, and human occupancy of said spaces shall be limited to that required to transport, arrange, and maintain stored materials.

22. Wholesaling, Storage and Distribution:
Light -- This use type refers to wholesaling, storage and warehousing services within enclosed structures. Typical uses include wholesale distributors, storage warehouses, or moving and storage firms.

23. Wholesaling, Storage and Distribution:
Heavy -- This use type refers to open air storage, distribution and handling of materials and equipment. Typical uses include monument or stone yards, grain elevators or open storage lumber distribution yards.

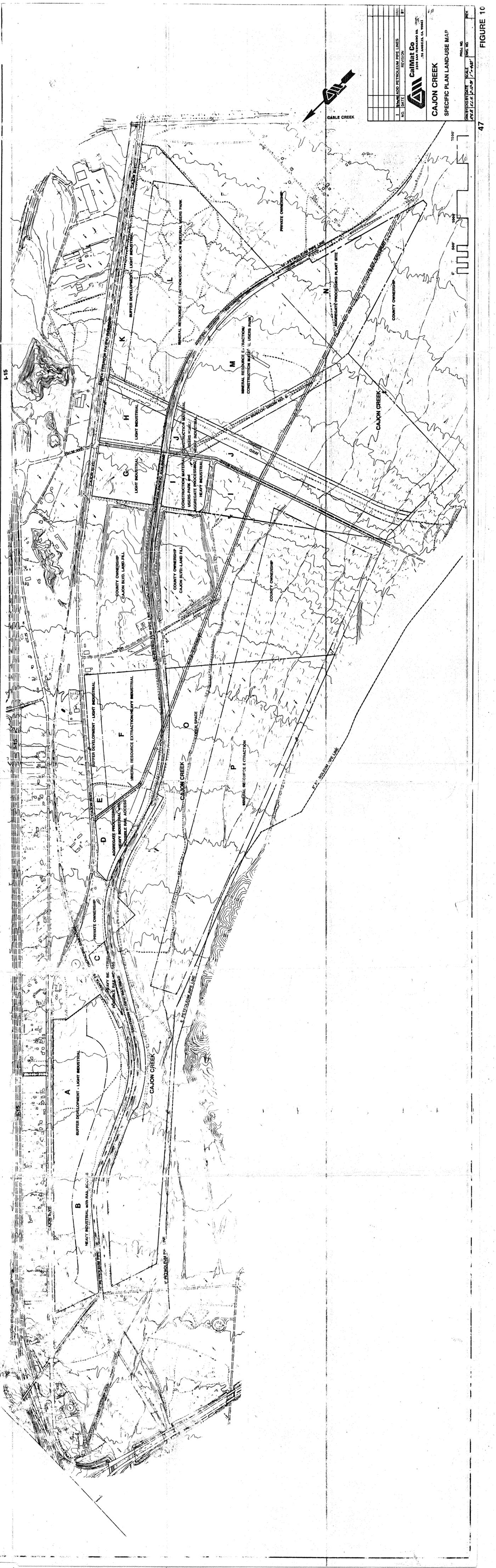
D. Specific Plan Map

A reduced-scale copy of the CalMat Cajon Creek Specific Plan land use map is shown in Figure 9. This land use map illustrates the land uses for each Planning Area. In summary, the Specific Plan ultimately provides for a total of 298 acres of industrial uses, 606 acres of mining-related uses and 488 acres of open space (Table 1).

TABLE 1

CALMAT CAJON CREEK SPECIFIC PLAN LAND USES

<u>Area</u>	<u>Land Use</u>	<u>Acres</u>
A	Buffer Development Light Industrial	77.0
B	Heavy Industrial with Rail Access	47.0
C	Heavy Industrial with Possible Rail Access	6.0
D	Heavy Industrial with Possible Rail Access/ Aggregate Processing Interim Use	18.5
E	Buffer Development Light Industrial	14.0
F	Heavy Industrial/Mineral Resource Extraction Interim Use	51.0
G	Light Industrial	26.0
H	Light Industrial	23.0
I	Heavy Industrial/Construction Material Users Park and Aggregate Processing Interim Use	36.5
J	Heavy Industrial/Construction Material Users Park	14.0
K	Buffer Development - Light Industrial	36.0
L	Mineral Resource Extraction/Construction Material Users Park	130.5
M	Mineral Resource Extraction/Construction Material Users Park	97.5
N	Aggregate Processing Plantsite	70.0
O	Open Space	488.0
P	Mineral Resource Extraction	257.0
	Total	<u>1392.0</u>



1-15



NO.	DATE	REVISION	BY
1		ADD PETROLEUM PIPE LINES	

CalMat Co
 2000 JAY RD. #100
 95043 SAN JOSE, CA 95131

CAJON CREEK
 SPECIFIC PLAN LAND-USE MAP

SCALE: 1" = 400'
 PROJECT NO.: 95-1-1-1-1-1
 DRAWING NO.: 1-1-1-1-1

E. Planning Areas

The CalMat Cajon Creek Specific Plan Area has been divided into 16 discrete units. These are referred to as Planning Areas A through P (Figure 9). A summary of allowable uses for each Planning Area is provided in Table 2. Definitions of these allowable land uses are set forth in Section IV.C. Planning Area Regulations for each of these areas, provided in Section V.C, specify land use guidelines, permitted uses and restricted uses, along with design guidelines, zoning and development standards.

E. Planning Areas

The CalMat Cajon Creek Specific Plan Area has been divided into 16 discrete units. These are referred to as Planning Areas A through P (Figure 9). A summary of allowable uses for each Planning Area is provided in Table 2. Definitions of these allowable land uses are set forth in Section IV.C. Planning Area Regulations for each of these areas, provided in Section V.C, specify land use guidelines, permitted uses and restricted uses, along with design guidelines, zoning and development standards.

Table 2

PLANNING AREA ALLOWABLE LAND USE SUMMARY

LAND USE TYPE	PLANNING AREA															
	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
				In/UL*		In/UL			In/UL	In/UL						
1. Automotive and Equip.: Fleet Storage	X	X	X	/X	X	X/X	X	X	X/X	X/X	X	X	X			
2. Automotive and Equip.: Heavy Equip. Repairs		X	X	X/				/X	/X	/X						
3. Automotive and Equip.: Light Equip. Repairs					X		X									
4. Automotive and Equip.: Heavy Equip. Sales/Rntls.		X	X	X/				/X	/X	/X						
5. Automotive and Equip.: Farm Equip. Sales/Rntls.		X	X	X/				/X	/X	/X						
6. Automotive and Equip.: Non-Oper. Vehicle Storage		X	X	X/				/X	/X	/X						
7. Automotive and Equip.: RV and Boat Storage		X	X	X/				/X	/X	/X						
8. Building Maintenance Services	X				X						X					
9. Business Equipment Sales and Services	X				X						X					
10. Construction Materials Users Activities						X/		X/	X/	X/		X				
11. Construction Sales and Services	X		X	X/	X			/X	/X	/X		X				
12. Contractor Storage Yards		X	X	X		X/		X/X	X/X	X/X		X				
13. Gasoline Sales							X									
14. General/Heavy Industrial		X	X	/X		/X		/X	/X	/X						
15. Manufacturing/Light Industrial	X	X	X	/X	X	/X	X	X	/X	/X	X					
16. Mineral Resource Extraction and Related Activities				X/		X/		X/	X/	/X	X	X	X	X		X
17. Open Space																
18. Recycling Collection Centers			X					/X	/X	/X						X
19. Repair Services, Consumer	X				X			/X	/X	/X						
20. Research and Development (R&D)	X				X			/X	/X	/X						
21. Wholesaling, Storage and Distrib.: Mini-Warehouse			X			X	X	/X	/X	/X						
22. Wholesaling, Storage and Distrib.: Light	X	X	X	/X	X	/X	X	/X	/X	/X						X
23. Wholesaling, Storage and Distrib.: Heavy		X	X	/X		/X		/X	/X	/X						/X

* In/UL = Interim/Ultimate

F. Project Phasing

The CalMat Cajon Creek Specific Plan is expected to be developed over an approximately 25 year period, although mineral resource extraction may continue beyond this time frame. Phasing, or development timing, has been placed into three categories: "Near Term", "Intermediate Term" and "Long Term". Within these categories are "Development Clusters", which indicate related development activity.

Near Term refers to foreseeable development or mineral resource-related activity which may occur during the early years of Specific Plan implementation. This includes placement of Planning Area O lands into Open Space, which will occur upon approval of the CalMat Cajon Creek Specific Plan. Mineral Resource Extraction, and Processing, is expected to occur within Planning Areas F and D, respectively, while Buffer Light Industrial development may occur in Planning Area E. Light Industrial development may also commence within Planning Areas G and H, depending upon market demand. Mineral Resource Extraction is expected to occur within Planning Area P, along with interim Aggregate Processing on a portion of Planning Area I. Construction Material Users Park activities may begin within Planning Areas L or M, depending on the timing of mining which will subsequently occur within these areas. Buffer Light Industrial development may occur within Planning Area K. Construction Material Users Park activities may also occur within Planning Areas I and J.

As the market for industrial land within the Specific Plan area continues to evolve, Intermediate Term development would be expected to occur, and Mineral Resource Extraction activities within Planning Areas L or M would commence. Related Aggregate Processing operations would then begin within Planning Area N. Depending on market demand, Planning Area I and J uses would be expected to grow into other interim and ultimate Heavy Industrial uses.

Long Term development within the Specific Plan area would occur as the market for industrial land matures, Near Term extractive and processing operations are completed, and interim uses are replaced with ultimate land uses. This would be expected to include Heavy Industrial development or redevelopment within Planning Areas B, C, D and F, Buffer Development Light Industrial uses in Planning Area A, and redevelopment of Planning Areas L and M with Construction Material Users Park and other compatible uses.

Table 3 summarizes these Specific Plan Development Phases. It is not the intent of this Specific Plan to restrict development to these phases, or to require completion of one phase prior to commencing the next. Actual development of land covered by this Specific Plan will be predicated upon the market for aggregate, as well as for industrial property. In addition, the development of industrial property is based on the timing and availability of necessary supportive infrastructure. Thus, the development phasing set forth in Table 3 is for planning purposes, and is not solely meant to direct project implementation.

TABLE 3

CALMAT CAJON CREEK SPECIFIC PLAN DEVELOPMENT PHASING

<u>Timing</u>	<u>Development Cluster</u>	<u>Planning Area and Land Use Designation</u>	<u>Planned Use**</u>	
Near Term (1991-1997) *	1a	O (SPOS) (PFC)	Open Space	
	1b	D (SPIE)	Aggregate Processing	
		F (SPIE)	Mineral Resource Extraction	
		E (SPIL)	Buffer Development - Light Industrial	
		1c	G (SPIL)	Light Industrial
			H (SPIL)	Light Industrial
	1d	I-part (SPIE)	Construction Material Users Park	
		J (SPIH)	Construction Material Users Park	
		P (SPIE)	Mineral Resource Extraction	
		I-part (SPIE)	Aggregate Processing	
	1e	L or M (SPIE)	Construction Material Users Park	
		K (SPIL)	Buffer Development - Light Industrial	

* For planning purposes only. This range of years is not meant to direct project implementation.

** These are examples of permitted uses that are currently planned to be carried out in each development cluster. For a complete list of permitted uses, see Planning Area Regulations.

TABLE 3 (continued)

Intermediate Term (1997-2007) *	2a	L (SPIE)	Mineral Resource Extraction
		M (SPIE)	Mineral Resource Extraction
		N (SPIE)	Aggregate Processing
Long Term (2007-2017) *	2b	I (SPIH)	Heavy Industrial - Redevelopment
		J (SPIH)	Heavy Industrial - Redevelopment
	3a	D (SPIH)	Heavy Industrial - Redevelopment
		F (SPIH)	Heavy Industrial - Redevelopment
3b		A (SPIL)	Buffer Development - Light Industrial
	B (SPIH)	Heavy Industrial with Rail Access	
	C (SPIH)	Heavy Industrial with possible Rail Access	
3c	L (SPIH)	Heavy Industrial/ Construction Material Users Park - Redevelopment	
	M (SPIH)	Heavy Industrial/ Construction Material Users Park - Redevelopment	

* For planning purposes only. This range of years is not meant to direct project implementation.

G. Infrastructure Improvement Plan

The purpose of the Infrastructure Improvement Plan (Appendix D) is to assure that necessary street improvements, railroad spurs, water, sewer and storm drain facilities are in place to support development as it occurs within the Specific Plan area.

As can be seen in Figure 10, Cajon Boulevard will ultimately have a street width of 64 feet curb-to-curb. As Planning Areas fronting on Cajon Boulevard develop, project side frontage improvements will be required as part of the Tentative Parcel Map process, in accordance with this Plan.

The intersection of Cajon Boulevard, Palm Avenue and Institution Road will require reconstruction to accommodate ultimate Specific Plan Area traffic. A major part of this reconstruction will consist of the realignment and widening of Palm Avenue and Institution Road as they approach the intersection. A traffic signal will also be required. The Infrastructure Improvement Plan also provides for minor improvement Cajon Boulevard/Kendall Drive intersection.

Future internal streets within Planning Areas A and B will be of a 50-foot curb-to-curb width. Internal streets within Planning Areas G, H, I and J will be 40 feet, in curb-to-curb width.

Because of necessary flexibility built into the Phasing Plan (Section III.E), the timing of roadway improvements may vary. In order to assure that roadway improvements will be in place when needed, the traffic study (Appendix C) set forth detailed improvements that will be necessary for each Planning Area, regardless of when development of that Planning Area occurs. A summary of these improvements is provided in Table 4.

The preliminary locations of anticipated railroad spurs are also shown in Figure 10.

The Infrastructure Improvement Plan (Appendix D) quantified water and sewer requirements of each Planning Area and, in similar fashion to street improvement requirements, set forth necessary water and sewer improvements which will be required on a Planning Area basis so that regardless of when a Planning Area develops, necessary infrastructure will be available. Table 5 contains a summary of these improvement requirements:

The Infrastructure Improvement Plan (Appendix D) also provides the approximate location and sizing for an underground storm drainage system designed to carry a 25-year frequency storm. Since there is no existing storm drainage system in the Specific

Plan area, various phases of project development will have to construct storm drains to accommodate the adjacent drainage basins. It will be necessary to obtain easements from the County of San Bernardino and the railroads in order to outlet these storm drains in Cajon Creek.

TABLE 4

Traffic Improvement Requirements

Near Term Development

<u>Planning Area</u>	<u>Use</u>	<u>Traffic Improvements Required</u>
D	Aggregate Processing	Cajon Boulevard Accel./ Decel. Lanes
F	Extraction	None
E	Light Industrial	Cajon Boulevard Frontage Improvements
G	Light Industrial	Cajon Boulevard/Institution Road Intersection Improvements Required. Institution Road/ Cajon Boulevard Frontage Improvements
H	Light Industrial	Cajon Boulevard, Institution Road Intersection Improvements Required. Institution Road/ Cajon Boulevard Frontage Improvements
I	Construction Mat. Users Park	None/Minor
J	Construction Mat. Users Park	None/Minor
K	Light Industrial	Frontage Improvements on Cajon Boulevard
L	Construction Mat. Users Park	None/Minor
M	Construction Mat. Users Park	None/Minor
P	Extraction	None/Minor
I	Aggregate Processing	None/Minor

TABLE 4 (continued)

Traffic Improvement Requirements

Intermediate Term Development*

<u>Planning Area</u>	<u>Use</u>	<u>Traffic Improvements Required</u>
L	Extraction	None
M	Extraction	None
N	Aggregate Processing	Access Rd./Institution Rd. Intersection Improvements
I	Heavy Industrial	Institution Road Frontage Improvements

Long-Term Development*

<u>Planning Area</u>	<u>Use</u>	<u>Traffic Improvements Required</u>
D	Heavy Industrial	None
F	Heavy Industrial Construction Material Users Park	None
A	Light Industrial	Cajon Blvd. Frontage Impr. Minor Impr. at Cajon Blvd./ Kendall Dr.
C	Heavy Industrial	Possible Cajon Blvd. Access Improvements
L	Heavy Industrial	None
M	Heavy Industrial	None

* Assumes Cajon Blvd./Palm Ave./Institution Road Intersection has been improved

TABLE 5

Water and Sewer Improvement Requirement Summary

WATER

<u>Planning Area</u>	<u>Improvements Required</u>
A,B	The existing 16-inch water main in Cajon Boulevard is adequate. An 8-inch water main will be required in the interior streets to distribute water to the lots.
C,D,E,F	Will be served directly from an existing 12-inch main in Cajon Boulevard.
G,H,I,J, K,L,	Service will be provided by either extending an existing 12-inch main in Cajon Boulevard located approximately 1200 feet southerly of the Specific Plan area, or by extending an existing 12-inch main in Cajon Boulevard southerly of Little League Drive. As Planning Areas G,H,I and J develop, a new 8-inch main will be extended down Institution Road. Additional 8-inch branches will be built in each of the cul-de-sac streets to distribute water to the interior lots.
N	Water for the permanent aggregate processing plant is expected to be supplied from an on-site metered well.

SEWER

A,B	An 8-inch sewer main will be constructed in interior streets to serve the lots.
E,K	Will be served directly from Cajon Boulevard
G,H,I,J	A new 8-inch main will be extended down Institution Road as these areas develop. Additional 8-inch branches of the main will be built in each of the cul-de-sac streets to serve the interior lots.
D,N	Sewer needs for aggregate processing plant uses will be met by chemical toilets or septic systems.

IMPLEMENTATION

V. IMPLEMENTATION

A. General Provisions

The CalMat Cajon Creek Specific Plan is intended to be implemented over a period of approximately 25 years. Implementation will be carried out either by the present owner of the privately owned land (CalMat), by future owners of land within this area, or by those who may acquire a leasehold interest.

The Implementation Regulations provided herein specify standards and procedures which will enable the Specific Plan to be implemented in compliance with both the goals of the project, and those of the City of San Bernardino General Plan. These regulations are to be followed in the design of individual development projects by owners and developers, and used in project approval review by the City of San Bernardino.

These Implementation Regulations differ from the standard zoning regulations of the City of San Bernardino in order to better direct future development in accordance with overall project goals. In order to remain in conformance with this Specific Plan, the development of property within each individual Planning Area or adjacent Planning Areas may require a cooperative effort among property owners or developers in the design and improvement of their properties. This will be necessary in order to assure that implementation of this Specific Plan results in the quality of development which was intended.

Development Review Procedures which are to be followed in the review and approval of proposed development plans have also been described. Because significant effort has been spent during the Specific Plan process in incorporating into the Specific Plan specific Planning Area Regulations and Design Guidelines for each Planning Area, approval of Development Permits will differ somewhat from the ordinance procedures set forth in Chapters 19.31 and 19.44 of the City's Development Code. Where specific development regulations are contained in the Specific Plan, those regulations shall apply. Where the Specific Plan is silent, the general regulations of the Development Code shall apply. The relationship between the Specific Plan and proposed Reclamation Plans for mined lands within the project must also be considered.

B. Development Review Procedures

Implementation of this Specific Plan will require a number of discretionary and ministerial reviews pursuant to the City of San Bernardino Development Code. It is specifically intended that implementation of portions of this Specific Plan within the Northwest Redevelopment Project be carried out by development review conducted by the City of San Bernardino Planning Department on behalf of the Redevelopment District. Standard applications and review procedures are intended to be followed, although special review procedures may be initiated if the Redevelopment Agency determines that such procedures will better accomplish Specific Plan goals. In addition, compliance with the Specific Plan Land Use Guidelines or Design Guidelines of each Planning Area may require submittal of plans and exhibits not normally required for development applications. These requirements and review procedures are described below:

1. General Controls and Limitations

All real property within the CalMat Cajon Creek Specific Plan area is subject to the controls and requirements of this Plan. No real property shall be developed, redeveloped, rehabilitated, or otherwise changed except in conformance with the provisions of this Specific Plan. The type, size, height, number and use of buildings within the Specific Plan area will be controlled by applicable City of San Bernardino Planning and Zoning Ordinances, the City's Development Code, and the provisions of the CalMat Cajon Creek Specific Plan, as they now exist or may hereafter from time-to-time be amended.

2. Zone Reclassification

Rezoning of the Specific Plan area will be done concurrently with the adoption of this Specific Plan, and the zoning regulations designated for each Planning Area shall be applied. Existing zoning will be replaced with the uses specified within this Specific Plan.

3. Subdivision

Subdivision of real property is regulated by State and local law. All subdivision maps filed pursuant to this Specific Plan will comply with the California Subdivision Map Act and the City of San Bernardino Development Code.

4. Development Permit

Approval of a Development Permit, consistent with the City of San Bernardino Development Code and as set forth herein, shall be required prior to the development of any use or structure, or the expansion or conversion of any existing use or structure permitted by this Specific Plan, except for those uses set forth in the Development Code which require approval of a Conditional Use Permit. Because significant effort has been spent at the Specific Plan level in incorporating specific Planning Area Regulations and Design Guidelines for each Planning Area into the Specific Plan, approval of Development Permits will differ somewhat from the ordinary procedures set forth in Chapters 19.31 and 19.44 of the City's Development Code.

a. Review Authority

Notwithstanding the provisions of Chapters 19.31 and 19.44, authority to review applications for Development Permits shall be as follows:

Development Permit

Review Authority

Industrial:

50,000+ square feet
1-50,000 square feet
Tenant Improvements

Development Review Committee
Planning Director
Planning Director

b. Findings

The City's approval of this Specific Plan, the accompanying Conditional Use Permit, and Environmental Impact Report have been made subject to the findings set forth in Section 1944.060 of the City's Development Code. Therefore, in lieu of the finding set forth in Section 19.44.060 of the City's Development Code, the review authority shall review Development Permit applications for consistency with the Specific Plan criteria set forth in the Planning Area Regulations and Design Guidelines.

c. Minor Modifications

When a nonsubstantive change in this Specific Plan, or in Conditional Use Permits or Development Permits issued hereunder is sought, the Director of Planning and Building Services, following consultation with other affected City Departments, may exercise reasonable discretion and approve such change without need for notice or hearing. For purposes of this section, "nonsubstantive" shall mean minor in scope and impact, and not a change which upon reasonable and careful examination by the Director is likely to lead to a meaningful change in environmental effects or an increase in density or intensity of use.

5. Conditional Use Permits

Conditional Use Permits shall be required for all mineral resource extraction and processing activities. All such Conditional Use Permits shall be reviewed and approved in accordance with the procedure set forth in Chapter 19.46 of the City of San Bernardino Development Code.

6. SMARA Reclamation Plans

A Reclamation Plan, pursuant to the requirements of the California Surface Mining and Reclamation Act (SMARA), as amended, shall be required for all mineral resource extraction and processing operations within the CalMat Cajon Creek Specific Plan area. All such Reclamation Plans shall be reviewed and approved in accordance with the procedures set forth in Chapter 19.60 of the City of San Bernardino Development Code.

7. Other Approvals

Normal ministerial review of grading plans, improvement plans and building permits would follow discretionary permit reviews in accordance with City of San Bernardino regulatory ordinances, except where those ordinances differ from the regulations specified in this Specific Plan.

C. Planning Area Regulations

The Planning Area Regulations contained within this Section specify Land Use Guidelines, Permitted Uses, Restricted Uses, Design Guidelines, Zoning and Development Standards for each of the Planning Areas within the CalMat Cajon Creek Specific Plan (Figure 9). It is the intent of this Specific Plan that permitted land uses, types of development and density, conform to those specified herein, which may differ from the conventional regulations of the City of San Bernardino Zoning Ordinance and Development Code. Other unspecified uses may be permitted if they are found to be similar to the identified uses, and will not significantly affect aggregate extraction or processing activities or other permitted uses. Such a determination shall be made by the Director of Planning and Building Services. For each Planning Area, the Permitted Uses, Restricted Uses, Zoning, and Development Standards contained in this Specific Plan shall be the regulations governing the permitted land uses, types of development, and development standards governing development in that Planning Area. Where the Specific Plan is silent, the general regulations of the Development Code shall apply.

Each Planning Area also contains Design Guidelines that supplement the Specific Plan Design Guidelines contained in Chapter VII of this Specific Plan. These Guidelines are intended as a reference framework to assist the designer in understanding the City's goals and objectives for high quality development within the Specific Plan area. The guidelines complement the mandatory site development regulations outlined above by providing good examples of potential design solutions and by providing design interpretations of the various mandatory regulations.

The Planning Area Design Guidelines and Specific Plan Design Guidelines are general and may be interpreted with some flexibility in their application to specific projects. The guidelines will be utilized during the City's design review process to encourage the highest level of design quality, while at the same time providing the flexibility necessary to encourage creativity on the part of project designers.

It should be understood that the size of each Planning Area, as noted herein, is based on planimetered calculations. As a result, any calculations of Gross Floor Area that may be made for Industrial Development Planning Areas, for example, should be considered to be estimates for planning purposes only, and are not intended to limit actual floor area for any proposed development.

The provisions of this Specific Plan shall be enforced in accordance with Chapter 19.52 of the City of San Bernardino Development Code and Government Code Section 65450 et. seq. Any violation of this Specific Plan shall constitute a violation of the City of San Bernardino Development Code and shall constitute an infraction.

All development applications shall be reviewed for compliance with the following standards and guidelines:

PLANNING AREA A

Size: 77.0 Acres.

Planned Use: Buffer Development - Light Industrial

Description. Planning Area A consists of about 77.0 acres located along the west side of Cajon Boulevard, approximately one-half mile south of Devore Road. Presently vacant, this relatively level parcel has approximately 4700 feet of frontage on Cajon Boulevard.

Land Use Guidelines. Planning Area A is intended to develop with smaller light industrial uses, with individual lot sizes ranging from one to two acres, which will be combinable. One of its purposes is to help "buffer" heavier, rail-oriented industrial development planned to occur within Planning Area B to the west. Light industrial uses are expected to consist of free-standing buildings of 10,000 to 40,000 s.f., and be occupied primarily by firms actively involved in light manufacturing and assembly and similar or compatible uses.

Planned land uses would consist of the following:

Permitted Uses

Automotive and Equipment: Fleet Storage
Building Maintenance Services
Business Equipment Sales and Services
Construction Sales and Services
Manufacturing/Light Industrial
Repair Services, Consumer
Research and Development
Wholesaling, Storage and Distribution: Light

Restricted Uses

Retail sales and services are permitted only as they relate to the permitted uses listed above. Direct sales to consumers of products produced, processed, packaged or warehoused on the premises are also permitted, provided that no retail sales or display of merchandise occurs outside the structures.

Outdoor storage shall be confined to the rear of the principal structure(s) or the rear two-thirds of the site, whichever is the more restrictive, and screened from view from any adjoining properties and public rights-of-way by appropriate walls, fencing and landscaping.

Fleet storage (Automotive and Equipment: Fleet Storage) shall be permitted only when directly related to another permitted use on the premises, or where a significant amount of on-site activity other than maintenance and repair, such as with parcel delivery or freight terminals, occurs within enclosed buildings. Automotive impound yards or wrecking yards shall not be permitted.

Supporting retail and personal service commercial uses are permitted provided that these uses constitute no more than 15 percent of the estimated total building square footage of the Planning Area.

Design Guidelines

Development of Planning Area A shall be in conformance with the CalMat Cajon Creek Specific Plan Design Guidelines.

The general development requirements contained in Chapter 19.20, Property Development Standards, and Chapter 19.30, Subdivision Standards, of the City of San Bernardino Development Code, shall also apply, except as otherwise provided for herein.

Particular attention shall be paid to architectural and landscape design, particularly as viewed from Cajon Boulevard and land uses to the east. A comprehensive landscaped or other suitable buffer, in keeping with Specific Plan Design Guidelines, shall be required along Cajon Boulevard. This buffer shall be similar in design, height and density to that required in Planning Area B.

Development within Planning Area A is encouraged to have access from the internal industrial collector street shared by Planning Area B, or other internal roads, rather than directly off Cajon Boulevard. The effects of direct driveway access to Cajon Boulevard should be minimized through the use of shared points of access.

Most buildings within Planning Area A should exhibit a common architectural theme. No steel buildings shall be permitted. Concrete tilt-up buildings and articulated building facades are encouraged. Zero lotline setbacks may be permitted if the cumulative total sidelot setback per parcel is maintained. Such lotline setbacks shall be shown to be feasible by means of a "Master Subdivision Plan" which shows feasible subdivision design and building placement and driveway location for all property within Planning Area A.

The setback from Cajon Boulevard shall be a minimum of 30 feet and used as a landscaped buffer. No parking should be permitted within the setback area. Vehicular parking should be visually attenuated from Cajon Boulevard and the industrial collector street shared with Planning Area B by means of a canopy of landscaping or other effective means, in keeping with Specific Plan Design Guidelines. Loading and truck parking should be at the rear of buildings, but may be permitted along the sides of buildings if it can be demonstrated through Development Permit Review that such activity will be effectively screened from view. Where perimeter walls or barriers are used, they should be of decorative block, landscaped earthen berms, or a combination thereof. The use of chain link fence shall be prohibited.

Comprehensive landscape and signage design is required. Open work areas shall not be permitted. Noise attenuation requirements for development along Cajon Boulevard shall be strictly adhered to.

Design of internal access roads shall be coordinated with the requirements of adjacent Planning Area B. Because of its length, design of the industrial collector street serving both Planning Area A and B shall be designed to discourage excessive speed, particularly with respect to approaches to curves.

Zoning

IL (Industrial Light)

Development Standards

Gross Lot Area (minimum)	20,000 s.f.
Floor Area Ratio (maximum) *	0.75 for "employee intensive" uses 1.0 for warehousing and "hardware intensive" uses
Front Setback (minimum)	30 feet
Rear Setback (minimum)	10 feet
Side Setback - each (minimum)	10 feet
Side Setback - street side (minimum)	10 feet
Lot Coverage (maximum)	75 percent
Structure Height (maximum)	2 stories (30 feet)**
Parking and Loading Requirements (minimum)	In accordance with Chapters 19.24 and 19.26 of the City of San Bernardino <u>Development Code</u> .
Landscape Setback	15 feet - Cajon Boulevard 10 feet - Interior Streets

* An intensity bonus of up to 12 square feet for each 1 square foot of permanent space for properly designed and administered day care facilities may be approved by the review authority.

** Where the maximum permitted height of a new structure exceeds 30 feet, the following provisions shall apply:

- a. Enhanced buffering to surrounding properties and the appropriateness of understructure parking shall be evaluated.
- b. A visual analysis relating structure proportions, massing, height and setback shall be conducted to preserve and enhance the scenic viewshed.
- c. The need and appropriateness of the additional height shall be demonstrated.
- d. Compatibility and harmony with surrounding development, and land use designations shall be demonstrated.
- e. Above 30 feet, additional structural setbacks (step back) may be required.

PLANNING AREA B

Size: 47.0 Acres

Planned Use: Heavy Industrial with Rail Access

Description. Planning Area B consists of approximately 47 acres. It is generally located between Cajon Boulevard and the Southern Pacific/Santa Fe/Union Pacific Railroad tracks, and extends from approximately 800 feet south of the Devore Road/Cajon Boulevard intersection to just north of the Kendall Drive intersection, except for Planning Area A which occupies the east central portion of this area. Its roadway frontage is shared with the "buffer" light industrial development to be developed within Planning Area A.

Land Use Guidelines. Planning Area B is intended to be developed with heavy manufacturing free-standing buildings of around 30,000 to 80,000 s.f. It is also expected to attract warehousing and distribution operations with individual buildings ranging in size from 80,000 to 150,000 s.f. Rail sidings and spurs would provide necessary rail access, with Cajon Boulevard and a new internal industrial collector street of 50 foot width within a 70 foot right-of-way, which would be shared with Area A, to provide necessary road access. Some light industrial uses, appropriate to this site, may be permitted.

Planned land uses would consist of the following:

Permitted Uses

Automotive and Equipment: Fleet Storage
General/Heavy Industrial
Manufacturing/Light Industrial
Wholesaling, Storage and Distribution: Light
Wholesaling, Storage and Distribution: Heavy

Restricted Uses

Retail sales and services are permitted only as they relate to the permitted uses listed above. Direct sales to consumers of products produced, processed, packaged or warehoused on the premises are also permitted, provided that no retail sale or display of merchandise occurs outside the structures.

Outdoor storage shall be screened from view from any adjoining properties and public rights-of-way by walls, fencing and landscaping, in accordance with Specific Plan Design Guidelines.

Fleet storage (Automotive and Equipment: Fleet Storage) shall be permitted only when directly related to another permitted use on the premises, or where a significant amount of on-site activity other than maintenance and repair, such as with parcel delivery or freight terminals, occurs within enclosed buildings. Automotive impound yards or wrecking yards shall not be permitted.

Supporting retail and personal service commercial uses are permitted provided that these uses constitute no more than 15 percent of estimated total building square footage of the Planning Area.

Design Guidelines

Development of Planning Area B shall be in conformance with the CalMat Cajon Creek Specific Plan Design Guidelines.

The general development requirements contained in Chapter 19.20, Property Development Standards, and Chapter 19.30, Subdivision Standards, of the City of San Bernardino Development Code, shall also apply, except as otherwise provided for herein.

Particular attention shall be paid to architectural and landscape design, especially as viewed from Cajon Boulevard and land uses to the east and north. A comprehensive landscaped or other suitable buffer, in keeping with Specific Plan Design Guidelines, shall be required along Cajon Boulevard. This buffer shall be similar in design, height and density to that required in Planning Area A.

Development within Planning Area B is encouraged to have access from the internal industrial collector street shared by Planning Area A, or other internal roads, rather than directly off Cajon Boulevard. The effects of direct driveway access to Cajon Boulevard should be minimized through the use of shared points of access.

Large lot users should be encouraged to locate within the broader, northern portion of Planning Area B. Particular attention should be paid to adequately buffering adjacent uses on the north. Rail-oriented users should be encouraged to locate within Planning Area B.

Most buildings within Planning Area B should exhibit a common architectural theme. No steel buildings shall be permitted. Concrete tilt-up buildings are encouraged. Vehicular parking or other on-site uses should be visually attenuated from Cajon Boulevard and the industrial collector street shared with Planning Area A by means of a canopy of landscaping or other effective means, in keeping with Specific Plan Design Guidelines. Loading, truck parking and rail sidings should be at the rear of buildings,

but may be permitted at other locations if it can be demonstrated through Development Permit Review that such activity is appropriate and can be effectively screened from external view.

Open work or storage areas are permitted with Development Permit Review. Such areas shall be screened from view from any adjoining properties, Cajon Boulevard and the industrial collector street by walls, fencing and landscaping, in keeping with Specific Plan Design Guidelines. Where perimeter walls or barriers are used, they should be of decorative block, landscaped earthen berms, or a combination thereof. Chain link fencing may be used along the rear two-thirds of lots, except along Cajon Boulevard or the industrial collector street, or as demonstrated appropriate through Development Permit Review. Where appropriate, such chain link fencing should be placed behind a landscaped setback, in accordance with Specific Plan Design Guidelines.

Comprehensive landscape and signage design is required. Noise attenuation requirements for development along Cajon Boulevard shall be strictly adhered to.

Design of internal access roads shall be coordinated with the requirements of adjacent Planning Area A. Because of its length, design of the industrial collector street serving both Planning Area A and B shall be designed to discourage excessive speed, particularly with respect to approaches to curves.

Zoning

IH (Industrial Heavy)

Development Standards

Gross Lot Area (minimum)	40,000 s.f.
Floor Area Ratio (maximum) *	0.75
Front Setback (minimum)	25 feet
Rear Setback (minimum)	10 feet
Side Setback - each (minimum)	10 feet
Side Setback - street side (minimum)	10 feet
Lot Coverage (maximum)	75 percent
Structure Height (maximum)	None
Parking and Loading Requirements (minimum)	In accordance with Chapters 19.24 and 19.26 of the City of San Bernardino <u>Development Code</u> .
Landscape Setback	15 feet - Cajon Boulevard 10 feet - Interior Streets

* An intensity bonus of up to 12 square feet for each 1 square foot of permanent space for properly designed and administered day care facilities may be approved by the review authority.

PLANNING AREA C

Size: 6.0 Acres

Planned Use: Heavy Industrial with Possible Rail Access

Description. Planning Area C is an approximately 6-acre triangularly-shaped parcel located immediately south of the divergence of the Santa Fe/Union Pacific and the Southern Pacific Railroads. It is presently a land-locked parcel, although prescriptive access rights are being researched to secure access across private ownership to Cajon Boulevard, south of the Santa Fe/Union Pacific Railroad overcrossing.

Land Use Guidelines. Planning Area C is intended to be developed with heavy industrial uses, which may include warehousing and distribution operations, in similar fashion to Planning Area B. If feasible, rail sidings and spurs may be provided. Some light industrial uses may be permitted which are in keeping with the small, isolated nature of the Planning Area.

Planned land uses would consist of the following:

Permitted Uses

Automotive and Equipment: Fleet Storage
Automotive and Equipment: Heavy Equipment Repairs
Automotive and Equipment: Heavy Equipment Sales/Rentals
Automotive and Equipment: Farm Equipment Sales/Rentals
Automotive and Equipment: Non-Operating Vehicle Storage
Automotive and Equipment: RV and Boat Storage
Building Maintenance Services
Construction Sales and Services
Contractor Storage Yards
General/Heavy Industrial
Manufacturing/Light Industrial
Recycling Collection Centers
Wholesaling, Storage and Distribution: Mini-Warehouses
Wholesaling, Storage and Distribution: Light
Wholesaling, Storage and Distribution: Heavy

Restricted Uses

Retail sales and services are permitted only as they relate to the permitted uses listed above. Direct sales to consumers of products produced, processed, packaged or warehoused on the premises are also permitted, provided that no retail sale or display of merchandise occurs outside the structures. Unscreened outdoor display of Heavy Equipment,

and Farm Equipment Sales and Rentals may be permitted within appropriate portions of lots, based on Development Permit review.

Outdoor storage shall be screened from view from any adjoining properties and public rights-of-way by walls, fencing and landscaping, in accordance with Specific Plan Design Guidelines.

Fleet storage (Automotive and Equipment: Fleet Storage) shall be permitted only when directly related to another permitted use on the premises, or where a significant amount of on-site activity other than maintenance and repair, such as with parcel delivery or freight terminals, occurs within enclosed buildings. Automotive impound yards may be permitted with an approved Conditional Use Permit from the City of San Bernardino. Automotive wrecking yards shall not be permitted.

Supporting retail and personal service commercial uses are permitted provided these uses constitute no more than 15 percent of the estimated total building square footage of the Planning Area.

Manufacturing/Light Industrial Uses may be permitted, providing that the Development Permit Review Process shows that they are compatible with other allowable uses.

Design Guidelines

Development of Planning Area C shall be in conformance with CalMat Cajon Creek Specific Plan Design Guidelines. The general development requirements contained in Chapter 19.20, Property Development Standards, and Chapter 19.30, Subdivision Standards, of the City of San Bernardino Development Code, shall also apply, except as provided for herein.

Rail-oriented uses of this area should be encouraged if it is determined that direct rail access is, in fact, feasible. One large, single exclusive use of the site should also be encouraged.

Prior to any development of Planning Area C, satisfactory access shall be secured to Cajon Boulevard. Such access should be compatible with intervening private ownership and should consider possible future rail spurs that might be placed in that area in the future.

Open storage and work areas are permitted, but must be in compliance with Specific Plan Design Guidelines. Because of Planning Area C's location between two above-grade railroads, chain link fencing may be used for security purposes on sides adjacent to the railroad embankment. Chain link fence may also

be utilized behind the front landscaped setback, however if a wall is utilized for this purpose, it should be of decorative block construction. Adequate buffering shall be provided to separate Planning Area C from land uses between it and Cajon Boulevard, in keeping with Specific Plan Design Guidelines.

Zoning

IH (Industrial Heavy)

Development Standards

Gross Lot Area (minimum)	40,000 s.f.
Floor Area Ratio (maximum)*	0.75
Front Setback (minimum)	25 feet
Rear Setback	10 feet
Side Setback - each (minimum)	10 feet
Side Setback - street side (minimum)	10 feet
Lot Coverage (maximum)	75 percent
Structure Height (maximum)	None
Parking and Loading Requirements (minimum)	In accordance with Chapters 19.24 and 19.26 of the City of San Bernardino <u>Development Code</u> .
Landscape Setback	10 feet - Interior Streets

* An intensity bonus of up to 12 square feet for each 1 square foot of permanent space for properly designed and administered day care facilities may be approved by the review authority.

PLANNING AREA D

Size: 18.5 Acres

Planned Use: Heavy Industrial with Possible Rail
Access/Aggregate Processing Interim Use

Description. Planning Area D is an approximately 18.5-acre irregularly-shaped parcel generally located between Cajon Boulevard and the Southern Pacific Railroad, approximately one-quarter mile south of the Santa Fe/Union Pacific Railroad overcrossing. The Southern California Edison overhead powerline easement forms Planning Area D's southeastern boundary.

Land Use Guidelines. Planning Area D is intended to be ultimately developed with heavy industrial uses, in similar fashion to Planning Area B. If feasible, future rail sidings and spurs may be provided. Some light industrial uses, appropriate to this site, may be permitted. Planning Areas D, E and F may be combined into one Tentative Map for subsequent industrial development purposes.

The primary interim use of Planning Area D will be to process rock, sand and gravel aggregate extracted either from adjacent Planning Area F or from Planning Area P, located within Cajon Creek to the west of the Southern Pacific Railroad. In the case of the latter, upon securing of permission from the railroad, a conveyor tunnel and/or vehicular undercrossing may be constructed under the railroad to allow movement of extracted material to Planning Area D for processing. A Conditional Use Permit will be required. Planning Areas D and F may be combined into one CUP. Other compatible uses may be permitted.

Planned land uses would consist of the following:

Permitted Uses - Interim

Mineral Resource Processing and Related Activities
Automotive and Equipment: Heavy Equipment Repairs
Automotive and Equipment: Heavy Equipment Sales/Rentals
Automotive and Equipment: Farm Equipment Sales/Rentals
Automotive and Equipment: Non-Operating Vehicle Storage
Automotive and Equipment: RV and Boat Storage
Construction Sales and Services
Contractor Storage Yards

Permitted Uses - Ultimate

Automotive and Equipment: Fleet Storage
General/Heavy Industrial
Manufacturing/Light Industrial
Wholesaling, Storage and Distribution: Light
Wholesaling, Storage and Distribution: Heavy

Restricted Uses - Interim

Mineral resource processing and related use activities include all uses permitted under the Mineral Resource Extraction and Processing and Related Activities use type. However, actual mineral resource extraction shall not occur within Planning Area D. Sand and gravel resource extracted within other portions of the Specific Plan area may be processed within Planning Area D. A Conditional Use Permit issued by the City of San Bernardino shall be required. All aggregate processing and related facilities shall be portable in nature, or readily removable upon completion of permitted activities.

Other permitted interim uses shall be allowed as long as mineral resource processing activities are conducted on-site, providing they are found to be compatible during the Development Permit process. Unscreened outdoor display associated with Heavy Equipment and Farm Equipment Sales and Rentals, and Construction Sales and Services may be permitted within appropriate portions of lots, based on Development Permit review.

Restricted Uses - Ultimate Uses

Retail sales and services are permitted only as they relate to the permitted ultimate uses listed above. Direct sales to consumers of products produced, processed, packaged or warehoused on the premises are also permitted, provided that no retail sale or display of merchandise occurs outside the structures.

Outdoor storage shall be screened from view from any adjoining properties and public rights-of-way by walls, fencing and landscaping, in accordance with Specific Plan Design Guidelines.

Fleet storage (Automotive and Equipment: Fleet Storage) shall be permitted only when directly related to another permitted use on the premises, or where a significant amount of on-site activity other than maintenance and repair, such as with parcel delivery or freight terminals, occurs within enclosed buildings. Automotive wrecking yards or impound yards shall not be permitted.

Supporting retail and personal service commercial uses are permitted provided that these uses constitute no more than 15 percent of the estimated total building square footage of the Planning Area.

Manufacturing/Light Industrial uses may be permitted, providing that the Development Permit process shows that they are compatible with other allowable uses.

Design Guidelines - Interim

Interim development of Planning Area D shall be in conformance with CalMat Cajon Creek Specific Plan Design Guidelines, as well as the requirements of a Conditional Use Permit issued by the City of San Bernardino for allowable mineral resource processing and related activities. All aggregate processing-related facilities shall be of a portable nature, or readily removable upon completion of such activities. A wet or dry processing plant may be permitted.

If practicable, aspects of the aggregate processing operation which are noisiest and most prominent in nature should be located away from Cajon Boulevard. Where feasible, material stockpiles should be used to attenuate noise of the processing plant from Cajon Boulevard or other development which may occur within Planning Area E. Appropriate landscape screening or other visual buffering, in keeping with Specific Plan Design Guidelines, shall be used along Cajon Boulevard frontage. Adequate access to Cajon Boulevard shall be provided for material transport trucks. An adequate supply of water shall be provided.

Design Guidelines - Ultimate

Ultimate development of Planning Area D, following mining-related usage, shall be in conformance with the CalMat Cajon Creek Specific Plan Design Guidelines. The general development requirements contained in Chapter 19.20, Property Development Standards, and Chapter 19.30, Subdivision Standards, of the City of San Bernardino Development Code, shall also apply, except as otherwise provided for herein.

A comprehensive landscaped or other suitable buffer, in keeping with Specific Plan Design Guidelines, shall be required along Cajon Boulevard frontage. This buffer shall be similar in design, height and density to that provided in Planning Area E.

Internal streets within Planning Area D should be coordinated with those of Planning Areas E and F. Internal street intersections with Cajon Boulevard should be limited. Development within Planning Area D is encouraged to have access from internal industrial collector streets, rather than directly off Cajon

Boulevard. The effects of direct individual lot driveway access to Cajon Boulevard should be minimized through the use of shared points of access.

Should direct rail access be determined feasible to serve Planning Areas C, D or F, larger lots should be considered. Rail-oriented uses should then be encouraged to locate within Planning Area D. Internal streets and lot layout within Planning Area D should be designed with respect to possible future rail access.

Open work or storage areas are permitted with Development Permit review. Such areas should be screened from view from any adjoining properties, Cajon Boulevard and internal streets by walls, fencing or landscaping, in keeping with Specific Plan Design Guidelines. Chain link fencing may be used in the rear two-thirds of lots, or where demonstrated appropriate through Development Permit review. Where appropriate, such chain link fencing should be placed behind a landscaped setback. Comprehensive landscape and signage design is required.

Zoning

IH (Industrial Heavy)

Development Standards

	<u>Interim</u>	<u>Ultimate</u>
	<u>Uses</u>	<u>Uses</u>
Gross Lot Area (minimum)	N/A	40,000 s.f.
Floor Area Ratio (maximum)	N/A	0.75*
Front Setback (minimum)	N/A	25 feet
Rear Setback (minimum)	N/A	10 feet
Side Setback - each (minimum)	N/A	10 feet
Side Setback - street side (minimum)	N/A	10 feet
Lot Coverage (maximum)	N/A	75 percent
Structure Height (maximum)	N/A	None
Parking and Loading Standards (minimum)	N/A	In accordance with Chapter 19.24 and 19.26 of the City of San Bernardino <u>Development Code.</u>
Landscape Setback	N/A	15 feet - Cajon Boulevard 10 feet - Interior Streets

* An intensity bonus of up to 12 square feet for each 1 square foot of permanent space for properly designed and administered day care facilities may be approved by the review authority.

PLANNING AREA E

Size: 14.0 Acres

Planned Use: Buffer Development - Light Industrial

Description. Planning Area E is a 250-foot wide, 14-acre area having an approximately one-half mile frontage along Cajon Boulevard. It is located immediately adjacent to Planning Area F on the west, which will be used for mineral resource extraction.

Land Use Guidelines. Planning Area E is intended to be developed with light industrial uses. One of its purposes is to "buffer" mineral resource extraction activities which will be located immediately west, in Planning Area F. As with Planning Area A, it is expected to be developed with free-standing buildings of 10,000 to 40,000 s.f., on approximately one-acre lots, which are combinable. Planning Areas D, E and F may be combined into one Tentative Map.

Planned land uses would consist of the following:

Permitted Uses

Automotive and Equipment: Fleet Storage
Building Maintenance Services
Business Equipment Sales and Services
Construction Sales and Services
Manufacturing/Light Industrial
Repair Services, Consumer
Research and Development
Wholesaling, Storage and Distribution: Light

Restricted Uses

Retail sales and services are permitted only as they relate to the permitted uses listed above. Direct sales to consumers of products produced, processed, packaged or warehoused on the premises are also permitted, provided that no retail sale or display of merchandise occurs outside the structures.

Outdoor storage shall be confined to the rear of the principal structure(s) or the rear two-thirds of the site, whichever is the more restrictive, and screened from view from any adjoining properties and public rights-of-way by appropriate walls, fencing and landscaping.

Fleet storage (Automotive and Equipment: Fleet Storage) shall be permitted only when directly related to another permitted use on the premises, or where a significant amount of on-site activity other than maintenance and repair, such as with parcel delivery or freight terminals, occurs within enclosed buildings. Automotive impound yards or wrecking yards shall not be permitted.

Supporting retail and personal service commercial uses are permitted provided that these uses constitute no more than 15 percent of the estimated building square footage of the Planning Area.

Design Guidelines

Development of Planning Area E shall be in conformance with CalMat Specific Plan Design Guidelines. The general development requirements contained in Chapter 19.20, Property Development Standards, and Chapter 19.30, Subdivision Standards, of the City of San Bernardino Development Code, shall also apply, except as otherwise provided for herein.

A comprehensive landscaped or other suitable buffer, in keeping with Specific Plan Design Guidelines, shall be required along Cajon Boulevard frontage. This buffer should be similar in design, height and density to that provided for in Planning Area D.

The effects of direct individual lot driveway access to Cajon Boulevard should be minimized through the use of shared points of access. Future internal streets within Planning Area E should be coordinated with those of Planning Areas D and F.

Most buildings within Planning Area E should exhibit a common architectural theme. Articulated building facades should be encouraged. No steel buildings shall be permitted. Concrete tilt-up buildings are also encouraged. Zero lot line setbacks may be permitted if the cumulative total side lot setback per parcel is maintained. Such lot line setbacks shall be shown to be feasible by means of a "Master Subdivision Plan" which shows feasible subdivision design, building placement and driveway location for all property within Planning Area E.

The setback from Cajon Boulevard shall be a minimum of 30 feet and used as a landscaped buffer. No parking should be permitted within the setback area. Vehicular parking should be visually attenuated from Cajon Boulevard and internal streets by means of a canopy of landscaping or other effective means, in keeping with Specific Plan Design Guidelines. The use of common driveways between lots is encouraged. Loading and truck parking should be at the sides of buildings, but may be permitted at the rear of buildings if it can be demonstrated through Development Permit

review that such activity will be effectively screened from view. Where perimeter walls or barriers are used, they should be of decorative block, landscaped earthen berms, or a combination thereof. The use of chain link fence shall not be permitted.

Comprehensive landscape and signage design is required. Open work areas shall not be permitted. Only uses which are compatible with mining activities within adjacent Planning Area F should be permitted.

Zoning

IL (Industrial Light)

Development Standards

Gross Lot Area (minimum)	20,000 s.f.
Floor Area Ratio (maximum)*	0.75 for "employee intensive" uses 1.0 for warehousing and "hardware intensive" uses
Front Setback (minimum)	25 feet
Rear Setback (minimum)	30 feet
Side Setback - each (minimum)	10 feet
Side Setback - street side (minimum)	10 feet
Lot Coverage (maximum)	75 percent
Structure Height (maximum)	2 stories (30 feet)**
Parking and Loading Standards (minimum)	In accordance with Chapters 19.24 and 19.26 of the City of San Bernardino <u>Development Code</u> .
Landscape Setback	15 feet - Cajon Boulevard 10 feet - Interior Streets

* An intensity bonus of up to 12 square feet for each 1 square foot of permanent space for properly designed and administered day care facilities may be approved by the review authority.

** Where the maximum permitted height of a new structure exceeds 30 feet, the following provisions shall apply:

- a. Enhanced buffering to surrounding properties and the appropriateness of understructure parking shall be evaluated.

- b. A visual analysis relating structure proportions, massing, height and setback shall be conducted to preserve and enhance the scenic viewshed.
- c. The need and appropriateness of the additional height shall be demonstrated.
- d. Compatibility and harmony with surrounding development, and land use designations shall be demonstrated.
- e. Above 30 feet, additional structural setbacks (step back) may be required.

PLANNING AREA F

Size: 51.0 Acres

Planned Use: Heavy Industrial/Mineral Resource Extraction
Interim Use

Description. Planning Area F is an approximately 51-acre parcel generally located between Cajon Boulevard and the Southern Pacific Railroad. It is located immediately east of Planning Area E. The Southern California Edison overhead powerline easement forms Planning Area F's northern and northwestern boundary.

Land Use Guidelines. Planning Area F is intended to be used for interim mineral resource extraction purposes. It will be buffered from surrounding land uses on the north by Planning Area D, where aggregate extracted from Planning Area F will be processed; by the above-grade Southern Pacific Railroad on the west; by the former County landfill and intervening light industrial land in other private ownership on the south; and by light industrial development within Planning Area E to the east. Access for non-aggregate truck-related traffic may be provided through Planning Areas D or E. Rail shipment of construction aggregate directly from Area F may occur, if determined feasible. Should extractive activities commence in Planning Area F prior to light industrial development occurring in adjacent Planning Area E, a combination of earthen berms, landscaping and/or setbacks may be used as an "interim" buffer. Planning Areas D and F may be combined within one Conditional Use Permit.

In concurrence with, or upon completion of extractive activities, portions of the extraction area may be filled with excess "fine" materials from the Planning Area D aggregate processing operation, or with suitable imported inert fill material, and used for open storage-type uses. Such filling shall be done in accordance with the City of San Bernardino Grading Ordinance, to result in a site which is suitable for subsequent building purposes, as provided for herein. Ultimate use of Planning Area F will be for heavy industrial uses, which may be rail-oriented. Planning Areas D, E and F may be combined into one Tentative Map.

Planned land uses would consist of the following:

Permitted Uses - Interim

Automotive and Equipment: Fleet Storage
Contractor Storage Yards
Construction Material Users Activities
Mineral Resource Extraction and Processing and Related Uses (with Conditional Use Permit and Reclamation Plan)

Permitted Uses - Ultimate

Automotive and Equipment: Fleet Storage
General/Heavy Industrial
Manufacturing/Light Industrial
Wholesaling, Storage and Distribution: Light
Wholesaling, Storage and Distribution: Heavy

Restricted Uses - Interim

Blasting associated with on-going mineral resource extraction activity shall not be permitted.

Retail Sales to consumers of Construction Material Users Park products produced, processed, packaged or warehoused on the premises are permitted.

Construction material users activities, contractor storage yards and automotive fleet storage are permitted, but shall utilize facilities which are portable in nature or are readily removable upon completion of their interim use of the site. Such uses shall be compatible with adjacent on-site aggregate-related operations and truck traffic. Automotive wrecking yards shall not be permitted.

Restricted Uses - Ultimate

Retail sales and services are permitted only as they relate to the permitted ultimate uses listed above. Direct sales to consumers of products produced, processed, packaged or warehoused on the premises are also permitted, provided that no retail sale or display of merchandise occurs outside the structures.

Outdoor storage shall be screened from view from any adjoining properties and public rights-of-way by walls, fencing and landscaping, in accordance with Specific Plan Design Guidelines.

Fleet storage (Automotive and Equipment: Fleet Storage) shall be permitted only when directly related to another permitted use on the premises, or where a significant amount of on-site activity other than maintenance and repair, such as with parcel delivery or freight terminals, occurs within enclosed buildings. Automotive wrecking yards or impound yards shall not be permitted.

Supporting retail and personal service commercial uses are permitted provided that these uses constitute no more than 15 percent of the estimated total building square footage of the Planning Area.

Manufacturing/Light Industrial Uses may be permitted, provided that the Development Permit review process shows that they are compatible with other allowable use.

Design Guidelines - Interim

Development of Planning Area F as a mineral resource extraction area shall be in conformance with the requirements of a Conditional Use Permit and Reclamation Plan approved by the City of San Bernardino. Both the CUP and the Reclamation Plan shall encompass both Planning Areas D and F, as both are directly related uses.

Mineral resource extracted within Planning Area F may be transported to and processed on adjacent Planning Area D. Primary processing may occur within the active extraction area in Planning Area F. Such activities shall be limited to primary crushing and/or removal of excess fines (scalping) prior to transport to the Planning Area D plantsite. An adequate supply of dust control water shall be provided.

Mining will be of the open quarry type. Extraction may occur to a depth of about 75 feet. Fifty-foot setbacks shall be observed on all sides of Planning Area F, except along the east side of the railroad embankment which contains the CalNev petroleum products pipeline, where a 100-foot setback will be observed. Peripheral slopes may be mined to a 1:1 slope and graded and reclaimed to a 2:1 slope ratio, in accordance with an approved Reclamation Plan.

Mining and reclamation shall be conducted in phases. Mining will first occur along the easterly portion of Planning Area F and progress, in phases, to the westerly portion of the site. All surface runoff from within the active extraction area shall be internally draining.

Planned light industrial development within adjacent Planning Area E should be used to "buffer" extractive operations within Planning Area F. Should commencement of extractive operations precede such buffer industrial development, earthen berms and/or landscape screen vegetation shall be used to accomplish the same objective. The entire Planning Area F active extractive operation shall be enclosed with a six-foot chain link fence for safety reasons.

Design Guidelines - Ultimate

Ultimate development of Planning Area F shall be in conformance with the CalMat Cajon Creek Specific Plan Design Guidelines. The general development requirements contained in Chapter 19.20, Property Development Standards, and Chapter 19.30, Subdivision Standards, of the City of San Bernardino Development Code shall also apply, except as otherwise provided for herein.

Upon completion of extractive activities and reclamation of portions of the site, re-use of Planning Area F may include development of light industrial uses along the eastern periphery, adjacent to Planning Area E. Development of these lots may be served by a common north-south industrial collector street service both Planning Areas E and F.

Upon completion of extractive operations and reclamation of the remainder of Planning Area F, on-going fill operations may continue to occur within the extraction area using suitable imported fill material. Such placement of fill shall be done in accordance with the City of San Bernardino Grading Ordinance, in a manner suitable for subsequent industrial uses, as provided for herein.

Should direct rail access be determined feasible to serve Planning Areas C, D or F, larger lots within the reclaimed extraction area should be considered, except along the eastern periphery unless suitably buffered. Rail-oriented uses should then be encouraged to locate within Planning Area F. Internal streets and lot layout within Planning Area F should be designed with respect to possible future rail access.

Open work or storage areas are permitted with Development Permit review. Such areas should be screened from view from any adjoining properties, light industrial development along eastern periphery, or adjacent streets by walls, fencing or landscaping, in keeping with Specific Plan Design Guidelines. Chain link fencing may be used in the rear two-thirds of lots, or where demonstrated appropriate through the Development Permit review process. Where appropriate, such chain link fencing should be placed behind a landscaped setback. Comprehensive landscape and signage design is required.

Zoning

IE (Industrial Extractive)

Development Standards

	<u>Interim Uses</u>	<u>Ultimate Uses</u>
Gross Lot Area (minimum)	N/A	40,000 s.f.
Floor Area Ratio (maximum)	N/A	0.75*
Front Setback (minimum)	N/A	25 feet
Rear Setback (minimum)	N/A	10 feet
Side Setback - each (minimum)	N/A	10 feet
Side Setback - street side (minimum)	N/A	10 feet
Lot Coverage (maximum)	N/A	75 percent
Structure Height (maximum)	N/A	None
Parking and Loading Standards (minimum)	N/A	In accordance with Chapters 19.24 and 19.26 of the City of San Bernardino <u>Development Code.</u>
Landscape Setback	N/A	10 feet - Interior Streets

* An intensity bonus of up to 12 square feet for each 1 square foot of permanent space for properly designed and administered day care facilities may be approved by the review authority.

PLANNING AREA G

Size: 26.0 Acres

Planned Use: Light Industrial

Description. Planning Area G is an approximately 26-acre parcel located immediately northwest of the Cajon Boulevard/Institution Road intersection. It is bounded on the north by the former County landfill, and on the west by the above-grade Southern Pacific Railroad.

Land Use Guidelines. Planning Area G is intended to be developed with light industrial uses. Such uses would be expected to consist of free-standing buildings of around 10,000 to 40,000 s.f. and occupied by firms actively involved with light manufacturing and assembly, and other compatible uses. Because of its high accessibility, portions of Planning Area G may be developed with gasoline service stations, convenience stores and similar uses. Although not provided for by this Specific Plan, consideration should be given to retaining approximately 10 acres of this area for future commercial use.

Planned land uses would consist of the following:

Permitted Uses

Automotive and Equipment: Fleet Storage
Automotive and Equipment: Light Equipment Repairs
Gasoline Sales
Manufacturing/Light Industrial
Research and Development
Wholesaling, Storage and Distribution: Mini-Warehouses
Wholesaling, Storage and Distribution: Light

Restricted Uses

Retail Sales and Services, except for Gasoline Sales uses, are permitted only as they relate to the permitted uses listed above. Direct sales to consumers of products produced, processed, packaged or warehoused on the premises are also permitted, provided that no retail sale or display of merchandise occurs outside the structures.

Outdoor storage shall be confined to the rear of the principal structure(s) or the rear two-thirds of the site, whichever is the more restrictive, and screened from view from any adjoining properties and public rights-of-way by appropriate walls, fencing and landscaping.

Fleet storage (Automotive and Equipment: Fleet Storage) shall be permitted only when directly related to another permitted use on the premises, or where a significant amount of on-site activity other than maintenance and repair, such as with parcel delivery or freight terminals, occurs within enclosed buildings. Automotive impound yards or wrecking yards shall not be permitted.

Supporting retail and personal service commercial uses except for Convenience Commercial uses, are permitted provided that these uses constitute no more than 15 percent of the estimated building square footage of the Planning Area.

Design Guidelines

Development of Planning Area G shall be in conformance with CalMat Cajon Creek Specific Plan Design Guidelines. The general development requirements contained in Chapter 19.20, Property Development Standards, and Chapter 19.30, Subdivision Standards, of the City of San Bernardino Development Code, shall also apply, except as otherwise provided for herein.

A comprehensive landscaped or other suitable buffer, in keeping with Specific Plan Design Guidelines, shall be required along both Cajon Boulevard and Institution Road, except where a specific land use is not conducive to the development of a continuous buffer. This buffer shall be similar in design, height and density to that required in Planning Area H.

Development within Planning Area G is encouraged to have frontage along internal roads, rather than directly off of Cajon Boulevard or Institution Road. The effects of direct driveway access to Cajon Boulevard or Institution Road should be minimized through the use of shared points of access.

Most of the buildings within Planning Area G should exhibit a common architectural theme. Articulated building facades should be encouraged. No steel buildings shall be permitted. Tilt-up concrete buildings are encouraged. Loading and truck parking should be at the rear of buildings, but may be permitted along the sides of buildings if it can be demonstrated through Development Permit review that such activity will be effectively screened from view. Where perimeter walls or barriers are used, they should be of decorative block, landscaped earthen berms, or a combination thereof. The use of chain link fence shall only be permitted along the rear or sides of lots adjacent to the former County landfill or the Southern Pacific Railroad embankment.

Comprehensive landscape and signage design is required. Open work areas shall not be permitted.

Zoning

IL (Industrial Light)

Development Standards

Gross Lot Area (minimum)	20,000 s.f.
Floor Area Ratio (maximum)*	0.75 for "employee intensive" uses 1.0 for warehousing and "hardware intensive" uses
Front Setback (minimum)	25 feet
Rear Setback (minimum)	10 feet
Side Setback - each (minimum)	10 feet
Side Setback - street side (minimum)	10 feet
Lot Coverage (maximum)	75 percent
Structure Height (maximum)	2 stories (30 feet)**
Parking and Loading Standards (minimum)	In accordance with Chapters 19.24 and 19.26 of the City of San Bernardino <u>Development Code</u> .
Landscape Setback	15 feet - Cajon Boulevard 10 feet - Institution Road 10 feet - Interior Streets

* An intensity bonus of up to 12 square feet for each 1 square foot of permanent space for properly designed and administered day care facilities may be approved by the review authority.

** Where the maximum permitted height of a new structure exceeds 30 feet, the following provisions shall apply:

- a. Enhanced buffering to surrounding properties and the appropriateness of understructure parking shall be evaluated.
- b. A visual analysis relating structure proportions, massing, height and setback shall be conducted to preserve and enhance the scenic viewshed.
- c. The need and appropriateness of the additional height shall be demonstrated.
- d. Compatibility and harmony with surrounding development, and land use designations shall be demonstrated.
- e. Above 30 feet, additional structural setbacks (step back) may be required.

PLANNING AREA H

Size: 23.0 Acres

Planned Use: Light Industrial

Description. Planning Area H is an approximately 23.0 acre area located immediately southwest of the Cajon Boulevard/Institution Road intersection. It is bounded on the south by the Metropolitan Water District and San Gabriel Valley Municipal Water District aqueducts, and on the west by the above-grade Southern Pacific Railroad.

Land Use Guidelines. Planning Area H is intended to be developed with light industrial uses. Such uses would be expected to consist of free-standing buildings of around 10,000 to 40,000 s.f. and occupied by firms actively involved with light manufacturing and assembly, or compatible uses. Because of its high accessibility, portions of Planning Area H may be developed with gasoline service stations, convenience stores and similar uses. Although not provided for by this Specific Plan, consideration should be given to retaining approximately 10 acres of this area for future commercial use.

Planned land uses would consist of the following:

Permitted Uses

Automotive and Equipment: Fleet Storage
Automotive and Equipment: Light Equipment Repairs
Gasoline Sales
Manufacturing/Light Industrial
Research and Development
Wholesaling, Storage and Distribution: Mini-Warehouses
Wholesaling, Storage and Distribution: Light

Restricted Uses

Retail sales and Services, except for and Gasoline Sales uses, are permitted only as they relate to the permitted uses listed above. Direct sales to consumers of products produced, processed, packaged or warehoused on the premises are also permitted, provided that no retail sale or display of merchandise occurs outside the structures.

Outdoor storage shall be confined to the rear of the principal structure(s) or the rear two-thirds of the site, whichever is the more restrictive, and screened from view from any adjoining properties and public rights-of-way by appropriate walls, fencing and landscaping.

Fleet storage (Automotive and Equipment: Fleet Storage) shall be permitted only when directly related to another permitted use on the premises, or where a significant amount of on-site activity other than maintenance and repair, such as with parcel delivery or freight terminals, occurs within enclosed buildings. Automotive impound yards or wrecking yards shall not be permitted.

Supporting retail and personal service commercial uses except for Convenience Commercial uses, are permitted provided that these uses constitute no more than 15 percent of the estimated total building square footage of the Planning Area.

Design Guidelines

Development of Planning Area H shall be in conformance with CalMat Cajon Creek Specific Plan Design Guidelines. The general development requirements contained in Chapter 19.20, Property Development Standards, and Chapter 19.30, Subdivision Standards, of the City of San Bernardino Development Code, shall also apply, except as otherwise provided for herein.

A comprehensive landscaped or other suitable buffer, in keeping with Specific Plan Design Guidelines, shall be required along both Cajon Boulevard and Institution Road, except where a specific land use is not conducive to the development of a continuous buffer. This buffer shall be similar in design, height and density to that required in Planning Area G.

Development within Planning Area H is encouraged to have frontage along internal roads, rather than directly off of Cajon Boulevard or Institution Road. The effects of direct driveway access to Cajon Boulevard or Institution Road should be minimized through the use of shared points of access.

Most of the buildings within Planning Area H should exhibit a common architectural theme. Articulated building facades should be encouraged. No steel buildings shall be permitted. Tilt-up concrete buildings are encouraged. Loading and truck parking should be at the rear of buildings, but may be permitted along the sides of buildings if it can be demonstrated through Development Permit review that such activity will be effectively screened from view. Where perimeter walls or barriers are used, they should be of decorative block, landscaped earthen berms, or a combination thereof. The use of chain link fence shall only be permitted along the rear or sides of lots adjacent to the Metropolitan Water District aqueduct easement or the Southern Pacific Railroad embankment.

Comprehensive landscape and signage design is required. Open work areas shall not be permitted.

Zoning

IL (Industrial Light)

Development Standards

Gross Lot Area (minimum)	20,000 s.f.
Floor Area Ratio (maximum)*	0.75 for "employee intensive" uses 1.0 for warehousing and "hardware intensive" uses
Front Setback (minimum)	25 feet
Rear Setback (minimum)	10 feet
Side Setback - each (minimum)	10 feet
Side Setback - street side (minimum)	10 feet
Lot Coverage (maximum)	75 percent
Structure Height (maximum)	2 stories (30 feet)**
Parking and Loading Standards (minimum)	In accordance with Chapters 19.24 and 19.26 of the City of San Bernardino <u>Development Code</u> .
Landscape Setback	15 feet - Cajon Boulevard 10 feet - Institution Road 10 feet - Interior Streets

* An intensity bonus of up to 12 square feet for each 1 square foot of permanent space for properly designed and administered day care facilities may be approved by the review authority.

** Where the maximum permitted height of a new structure exceeds 30 feet, the following provisions shall apply:

- a. Enhanced buffering to surrounding properties and the appropriateness of understructure parking shall be evaluated.
- b. A visual analysis relating structure proportions, massing, height and setback shall be conducted to preserve and enhance the scenic viewshed.
- c. The need and appropriateness of the additional height shall be demonstrated.
- d. Compatibility and harmony with surrounding development, and land use designations shall be demonstrated.
- e. Above 30 feet, additional structural setbacks (step back) may be required.

PLANNING AREA I

Size: 36.5 Acres

Planned Use: Heavy Industrial/Construction Materials Users Park
and Aggregate Processing Interim Use

Description. Planning Area I is an approximately 36.5-acre parcel located north of Institution Road and west of the Southern Pacific Railroad. It is bounded on the north by the former County land-fill and the Southern California Edison powerline easement on the west.

Land Use Guidelines. Planning Area I is intended to be developed on an interim basis as a Construction Materials Users Park (CMUP). Construction material users are expected to include a wide variety of businesses tied to the construction industry, such as concrete block manufacturing, roofing materials, concrete pipe, etc. The primary attraction for these businesses will be the ready and convenient access to construction aggregate materials from CalMat for the manufacture of concrete products and the like. Contractor storage yards and automotive fleet storage, compatible with interim uses of Planning Area I, may also be permitted.

A full range of aggregate (rock, sand and gravel) processing activities may also occur within Planning Area I on an interim basis. Such a processing operation, which would be portable in nature, could process material extracted from the portion of the 100-year floodplain of Cajon Creek located within Planning Area P, or other Specific Plan extraction areas. Other related aggregate processing activities could occur, including the re-crushing of imported portland cement concrete and asphaltic concrete, although no mining will be conducted within Planning Area I. A Conditional Use Permit will be required for on-site aggregate processing uses. Such aggregate processing uses should be located to complement Construction Material Users Park (CMUP) or other uses within this Planning Area.

Ultimate uses within Planning Area I, which are planned to occur upon completion of interim aggregate processing, CMUP, or other allowable uses, may be phased in at any point in time, provided they are compatible with such interim uses, provide proper buffering to attenuate noise, visual and other concerns related to the adjacent interim uses, and meet all requirements of the Specific Plan Design Guidelines. Some light industrial uses, appropriate to this site, should be permitted.

Planning Area I includes lands that are presently within a portion of the FEMA-mapped 100-year floodplain. However, hydrologic study (Appendix F) has indicated that Planning Area I (as shown in

Figure 9) is not within the actual 100-year floodplain. No development activity as provided for herein, except for mineral resource-related haul roads, shall occur within the presently-mapped 100-year FEMA floodplain within Planning Area I until the FEMA floodplain map has been officially revised.

Planned land uses would consist of the following:

Permitted Uses - Interim

Automotive and Equipment: Fleet Storage
Construction Material Users Activities
Contractor Storage Yards
Mineral Resource Processing and Related Activities

Permitted Uses - Ultimate

Automotive and Equipment: Fleet Storage
Automotive and Equipment: Heavy Equipment Repairs
Automotive and Equipment: Heavy Equipment Sales/Rentals
Automotive and Equipment: Farm Equipment Sales/Rentals
Automotive and Equipment: Non-Operating Vehicle Storage
Automotive and Equipment: RV and Boat Storage
Construction Sales and Services
Contractor Storage Yards
General/Heavy Industrial
Manufacturing/Light Industrial
Recycling Collection Centers
Research and Development
Wholesaling, Storage and Distribution: Light
Wholesaling, Storage and Distribution: Heavy

Restricted Uses - Interim

Mineral resource processing and related use activities include all uses permitted under the Mineral Resource Extraction and Processing and Related Activities use type. However, actual mineral resource extraction shall not occur within Planning Area I. Rock, sand and gravel resources extracted within other portions of the Specific Plan area may be processed within Planning Area I. A Conditional Use Permit issued by the City of San Bernardino shall be required. All aggregate processing and related facilities shall be portable in nature, or readily removable upon completion of permitted activities. An adequate supply of dust control water shall be provided.

Construction material users activities, and contractor storage yards, and automotive fleet storage are permitted, but shall utilize facilities that are portable in nature or are readily removable upon completion of their interim use of the

site. Such uses shall be compatible with adjacent on-site aggregate-related operations and truck traffic. Automotive wrecking yards shall not be permitted.

Retail Sales to consumers of Construction Materials Users Park products produced, processed, packaged or warehoused on the premises are permitted.

Restricted Uses - Ultimate

Retail sales and services are permitted only as they relate to the permitted ultimate uses listed above. Direct sales to consumers of products produced, processed, packaged or warehoused on the premises are also permitted, provided that no retail sale or display of merchandise occurs outside the structures. Unscreened outdoor display associated with Heavy Equipment, Farm Equipment Sales and Rentals, and Construction Sales and Services, may be permitted within appropriate portions of lots, based on Development Permit review.

Outdoor storage shall be screened from view from any adjoining properties and public rights-of-way by walls, fencing and landscaping, in accordance with Specific Plan Design Guidelines.

Fleet storage (Automotive and Equipment: Fleet Storage) shall be permitted only when directly related to another permitted use on the premises, or where a significant amount of on-site activity other than maintenance and repair, such as with parcel delivery or freight terminals, occurs within enclosed buildings. Automotive wrecking yards shall not be permitted.

Supporting retail and personal service commercial uses are permitted provided that these uses constitute no more than 15 percent of the estimated building square footage of the Planning Area.

Design Guidelines - Interim

Interim development of Planning Area I shall be in conformance with CalMat Cajon Creek Specific Plan Design Guidelines, as well as the requirements of a Conditional Use Permit issued by the City of San Bernardino for allowable mineral resource processing and related activities (if conducted). All aggregate processing-related facilities, or facilities for other interim uses, shall be of a portable nature, or readily removable upon completion of such activities.

As practicable, aspects of the aggregate processing operation which are noisiest and most prominent in nature should be located away from Institution Road and the Cajon Creek FEMA 100-year

floodplain. More prominent plantsite features should be backdropped against the Southern Pacific Railroad embankment. Where feasible, material stockpiles should be used to attenuate noise of the processing plant from areas further west. Appropriate landscape screening or other visual buffering, in keeping with Specific Plan Design Guidelines, shall be used along Institution Road frontage, and along portions of the western boundary to attenuate views from the west. Adequate access to Institution Road shall be provided for aggregate material transport trucks.

Open storage and work areas shall be permitted. Such areas shall be screened from view from any non-similar adjoining properties, Institution Road, and views from Cajon Creek by walls, fencing and landscaping, in keeping with Specific Plan Design Guidelines. Chain link fencing may be used, provided it is used in conjunction with a landscaped buffering and is not visible from outside the Planning Area.

Design Guidelines - Ultimate

Ultimate development of Planning Area I shall be in conformance with the CalMat Cajon Creek Specific Plan Design Guidelines. The general development requirements contained in Chapter 19.20, Property Development Standards, and Chapter 19.30, Subdivision Standards, of the City of San Bernardino Development Code, shall also apply, except as otherwise provided for herein.

A comprehensive landscaped or other suitable buffer, in keeping with Specific Plan Design Guidelines, shall be required along Institution Road frontage, as well as along the western boundary of Planning Area I in order to minimize intrusion into Planning Area O open space uses within the Cajon Creek 100-year floodplain. This buffer shall be similar in design, height and density to that provided in Planning Area J.

Internal street access to Planning Area I should be limited to two points along Institution Road frontage and should be coordinated with that of Planning Area J. Open work or storage areas are permitted with Development Permit Review. Such areas should be screened from view from any adjoining properties, Institution Road, the Cajon Creek area, and internal streets by walls, fencing or landscaping, in keeping with Specific Plan Design Guidelines.

Chain link fencing may be used in the rear two-thirds of lots, or where demonstrated appropriate through Development Permit Review. Where appropriate, such chain link fencing should be placed behind a landscaped setback. Comprehensive landscape and signage design is required.

Zoning

IH (Industrial Heavy)

Development Standards

	<u>Interim</u> <u>Uses</u>	<u>Ultimate</u> <u>Uses</u>
Gross Lot Area (minimum)	N/A	40,000 s.f.
Floor Area Ratio (maximum)	N/A	0.75*
Front Setback (minimum)	N/A	25 feet
Rear Setback (minimum)	N/A	10 feet
Side Setback - each (minimum)	N/A	10 feet
Side Setback - street side (minimum)	N/A	10 feet
Lot Coverage	N/A	75 percent
Structure Height (maximum)	N/A	None
Parking and Loading Standards (minimum)	N/A	In accordance with Chapters 19.24 and 19.26 of the City of San Bernardino <u>Development Code.</u>
Landscape Setback	N/A	10 feet - Institution Road 10 feet - Interior Streets

* An intensity bonus of up to 12 square feet for each 1 square foot of permanent space for properly designed and administered day care facilities may be approved by the review authority.

PLANNING AREA J

Size: 14.0 Acres

Planned Use: Heavy Industrial/Construction Material Users Park

Description. Planning Area J is an approximately 14.0-acre parcel located south of Institution Road, west of the Southern Pacific Railroad, north of the Metropolitan Water District and San Gabriel Valley Municipal Water District aqueducts. It is bounded on the west by the Southern California Edison powerline easement.

Land Use Guidelines. Planning Area J is intended to be developed on an interim basis as a Construction Material Users Park (CMUP), utilizing the convenient access to construction aggregate materials from CalMat for the manufacture of concrete products and the like. The re-crushing of imported portland cement concrete and asphaltic concrete should also be permitted as an interim use. Ultimately, Planning Area J is planned to be utilized for heavy industrial uses. Contractor storage yards and automotive fleet storage, compatible with interim uses of Planning Area J, may also be permitted.

Ultimate uses within Planning Area J, which are planned to occur upon completion of interim Construction Material Users Park, contractor storage yard, or other interim uses, may be phased in provided they are compatible with such interim uses, provide proper buffering to attenuate noise, visual and other concerns related to the adjacent uses, and meet all requirements of the Specific Plan Design Guidelines. Some light industrial uses, appropriate to this site, should be permitted.

Planning Area J includes lands that are presently within a portion of the FEMA-mapped 100-year floodplain. However, hydrologic study (Appendix F) has indicated that Planning Area J (as shown in Figure 9) is not within the actual 100-year floodplain. No development activity as provided for herein, except for mineral resource-related haul roads, shall occur within the presently-mapped 100-year FEMA floodplain until the FEMA floodplain map has been officially revised.

Planned land uses would consist of the following:

Permitted Uses - Interim

Contractor Storage Yards
Construction Material Users Activities
Automotive and Equipment: Fleet Storage

Permitted Uses - Ultimate

Automotive and Equipment: Fleet Storage
Automotive and Equipment: Heavy Equipment Repairs
Automotive and Equipment: Heavy Equipment Sales/Rentals
Automotive and Equipment: Farm Equipment Sales/Rentals
Automotive and Equipment: Non-Operating Vehicle Storage
Automotive and Equipment: RV and Boat Storage
Construction Sales and Services
Contractor Storage Yards
General/Heavy Industrial
Manufacturing/Light Industrial
Recycling Collection Centers
Research and Development
Wholesaling, Storage and Distribution: Light
Wholesaling, Storage and Distribution: Heavy

Restricted Uses - Interim

Construction material users activities, including the re-crushing of imported portland cement concrete and asphaltic concrete, and contractor storage yards, are permitted, but shall utilize facilities that are portable in nature or are readily removable upon completion of their interim use of the site. Such uses shall be compatible with adjacent on-site access road aggregate-related truck traffic from Planning Areas M and N. Automotive wrecking yards shall not be permitted.

Retail sales to consumers of Construction Materials Users Park products produced, processed, packaged or warehoused on the premises are permitted.

Restricted Uses - Ultimate

Retail sales and services are permitted only as they relate to the permitted uses listed above. Direct sales to consumers of products produced, processed, packaged or warehoused on the premises are also permitted, provided that no retail sale or display of merchandise occurs outside the structures. Unscreened outdoor display associated with Heavy Equipment and Farm Equipment Sales and Rentals, and Construction Sales and Services, may be permitted within appropriate portions of lots, based on Development Permit review.

Outdoor storage shall be screened from view from any adjoining properties and public rights-of-way by walls, fencing and landscaping, in accordance with Specific Plan Design Guidelines.

Fleet storage (Automotive and Equipment: Fleet Storage) shall be permitted only when directly related to another permitted use on the premises, or where a significant amount of on-site activity other than maintenance and repair, such as with parcel delivery or freight terminals, occurs within enclosed buildings. Automotive wrecking yards shall not be permitted.

Supporting retail and personal service commercial uses are permitted provided that these uses constitute no more than 15 percent of the estimated building square footage of the Planning Area.

Manufacturing/Light Industrial Uses may be permitted, providing that the Development Permit review process shows that they are compatible with other allowable uses.

Design Guidelines - Interim

Interim development of Planning Area J shall be in conformance with CalMat Cajon Creek Specific Plan Design Guidelines. All interim use facilities shall be of a portable nature or readily removable upon completion of such activities.

Appropriate landscape screening or other visual buffering, in keeping with Specific Plan Design Guidelines, shall be used along Institution Road frontage, and along portions of the western boundary to attenuate views from the west. Adequate access shall be provided for both on-site uses, as well as the provision of an on-site access road for aggregate material transport trucks connecting with Planning Areas M and N to the south. Such access shall be coordinated with that of Planning Area I.

Open storage and work areas shall be permitted. Such areas shall be screened from view from any non-similar adjoining properties, Institution Road, and views from Cajon Creek by walls, fencing and landscaping, in keeping with Specific Plan Design Guidelines. Chain link fencing may be used, provided it is used in conjunction with a landscaped buffering and is not visible from outside the Planning Area.

Design Guidelines - Ultimate

Ultimate development of Planning Area J shall be in conformance with CalMat Cajon Creek Specific Plan Design Guidelines. The general development requirements contained in Chapter 19.20, Property Development Standards, and Chapter 19.30, Subdivision Standards, of the City of San Bernardino Development Code, shall also apply except as otherwise provided for herein.

A comprehensive landscaped or other suitable buffer, in keeping with Specific Plan Design Guidelines, shall be required along Institution Road frontage, as well as along the western boundary of Planning Area J, in order to minimize intrusion into Planning Area O open space uses within the Cajon Creek 100-year floodplain. This buffer shall be similar in design, height and density to that provided in Planning Area I.

Internal street access to Planning Area J should be limited to two points along Institution Road frontage and should be coordinated with that of Planning Areas I and M. Open work or storage areas are permitted with Development Permit review. Such areas should be screened from view from any adjoining properties, Institution Road, the Cajon Creek area, and internal streets by walls, fencing or landscaping, in keeping with Specific Plan Design Guidelines.

Chain link fencing may be used in the rear two-thirds of lots, or as demonstrated appropriate through Development Permit review. Where appropriate, such chain link fencing should be placed behind a landscaped setback. Comprehensive landscape and signage design is required.

Zoning

IH (Industrial Heavy)

Development Standards

Gross Lot Area (minimum)	40,000 s.f.
Floor Area Ratio (maximum)	0.75*
Front Setback (minimum)	25 feet
Rear Setback (minimum)	10 feet
Side Setback - each (minimum)	10 feet
Side Setback - street side (minimum)	10 feet
Lot Coverage (maximum)	75 percent
Structure Height (maximum)	None
Parking and Loading Requirements (minimum)	In accordance with Chapters 19.24 and 19.26 of the City of San Bernardino <u>Development Code</u>
Landscape Setback	10 feet - Institution Road 10 feet - Interior Streets

* An intensity bonus of up to 12 square feet for each 1 square foot of permanent space for properly designed and administered day care facilities may be approved by the review authority.

PLANNING AREA K

Size: 36.0 Acres

Planned Use: Buffer Development - Light Industrial

Description. Planning Area K is an approximately 36-acre area which fronts on Cajon Boulevard for a distance of about 0.8 mile, from just south of the Metropolitan Water District and San Gabriel Valley Municipal Water District aqueducts to just north of the Cable Creek Channel.

Land Use Guidelines. Planning Area K is intended to be developed with light industrial uses, with individual lot sizes ranging from one to two acres, which will be combinable. Light industrial uses are expected to consist of free-standing buildings of 10,000 to 40,000 s.f., and be occupied primarily by firms actively involved in light manufacturing and assembly or compatible uses. A larger lot, if placed at the southern end of this Planning Area, may be used for larger, more generalized industrial use appropriate to its size and location. Another purpose for Planning Area K's linear light industrial development is to provide a permanent buffer for mineral resource extraction operations to the west which will occur over the long term.

Planned land uses would consist of the following:

Permitted Uses

Automotive and Equipment: Fleet Storage
Building Maintenance Services
Business Equipment Sales and Services
Construction Sales and Services
Manufacturing/Light Industrial
Repair Services, Consumer
Research and Development
Wholesaling, Storage and Distribution: Light

Restricted Uses

Retail sales and services are permitted only as they relate to the permitted uses listed above. Direct sales to consumers of products produced, processed, packaged or warehoused on the premises are also permitted, provided that no retail sale or display of merchandise occurs outside the structures. Un-screened outdoor display associated with Construction Sales and Services may be permitted within appropriate portions of lots, based on Development Permit review.

Outdoor storage shall be confined to the rear of the principal structure(s) or the rear two-thirds of the site, whichever is the more restrictive, and screened from view from any adjoining properties and public rights-of-way by appropriate walls, fencing and landscaping.

Fleet storage (Automotive and Equipment: Fleet Storage) shall be permitted only when directly related to another permitted use on the premises, or where a significant amount of on-site activity other than maintenance and repair, such as with parcel delivery or freight terminals, occurs within enclosed buildings. Automotive impound yards or wrecking yards shall not be permitted.

Supporting retail and personal service commercial uses are permitted provided that these uses constitute no more than 15 percent of the estimated building square footage of the Planning Area.

Design Guidelines

Development of Planning Area K shall be in conformance with CalMat Specific Plan Design Guidelines. The general development requirements contained in Chapter 19.20, Property Development Standards, and Chapter 19.30, Subdivision Standards, of the City of San Bernardino Development Code, shall also apply, except as otherwise provided for herein.

A comprehensive landscaped or other suitable buffer, in keeping with Specific Plan Design Guidelines, shall be required along Cajon Boulevard frontage. This buffer should be similar in design, height and density to that provided for in Planning Area H. A similar buffer shall be required along the southerly portion of Planning Area K.

The effects of direct individual lot driveway access to Cajon Boulevard should be minimized through the use of shared points of access. Consideration should be given to possible shared usage of an internal collector street with light industrial lots which could, in the future, result from reclamation of the easterly portion of the Planning Area L mining area. Internal streets within Planning Area K should be coordinated with those of Planning Areas H and L.

Most buildings within Planning Area K should exhibit a common architectural theme. Articulated building facades should be encouraged. No steel buildings shall be permitted. Concrete tilt-up buildings are recommended. Zero lot line setbacks may be permitted if the cumulative total lot setback per parcel is maintained. Such lot line setbacks shall be shown to be feasible

by means of a "Master Subdivision Plan" which shows feasible subdivision design, building placement and driveway location for all property within Planning Area K.

The setback from Cajon Boulevard shall be a minimum of 30 feet and used as a landscaped buffer. No parking should be permitted within the setback area. Vehicular parking should be visually attenuated from Cajon Boulevard and internal streets by means of a canopy of landscaping or other effective means, in keeping with Specific Plan Design Guidelines. The use of common driveways between lots is encouraged. Loading and truck parking should be at the sides of buildings, but may be permitted at the rear of buildings if it can be demonstrated through Development Permit review that such activity will be effectively screened from view. Where perimeter walls or barriers are used, they should be of decorative block, landscaped earthen berms, or a combination thereof. The use of chain link fence shall not be permitted.

Comprehensive landscape and signage design is required. Open work areas shall not be permitted. Only uses which are compatible with long-term mining activities within adjacent Planning Area L should be permitted.

Zoning

IL (Industrial Light)

Development Standards

Gross Lot Area (minimum)	20,000 s.f.
Floor Area Ratio (maximum)*	0.75 for "employee intensive" uses 1.0 for warehousing and "hardware intensive" uses
Front Setback (minimum)	25 feet
Rear Setback (minimum)	30 feet
Side Setback - each (minimum)	10 feet
Side Setback - street side (minimum)	10 feet
Lot Coverage (maximum)	75 percent
Structure Height (maximum)	2 stories (30 feet)**
Parking and Loading Standards (minimum)	In accordance with Chapters 19.24 and 19.26 of the City of San Bernardino <u>Development Code</u> .
Landscape Setback	15 feet - Cajon Boulevard 10 feet - Interior Streets

* An intensity bonus of up to 12 square feet for each 1 square foot of permanent space for properly designed and administered day care facilities may be approved by the review authority.

** Where the maximum permitted height of a new structure exceeds 30 feet, the following provisions shall apply:

- a. enhanced buffering to surrounding properties and the appropriateness of understructure parking shall be evaluated.
- b. A visual analysis relating structure proportions, massing, height and setback shall be conducted to preserve and enhance the scenic viewshed.
- c. The need and appropriateness of the additional height shall be demonstrated.
- d. Compatibility and harmony with surrounding development, and land use designations shall be demonstrated.
- e. Above 30 feet, additional structural setbacks (step back) may be required.

PLANNING AREA L

Size: 130.5 Acres

Planned Use: Mineral Resource Extraction/Construction
Material Users Park/Ultimate Heavy Industrial

Description. Planning Area L is an approximately 130.5-acre parcel bounded by the Metropolitan Water District and San Gabriel Valley Municipal Water District aqueducts on the north, the Southern Pacific Railroad on the west, private ownership on the south, and Planning Area K on the east.

Land Use Guidelines. Planning Area L is intended to be used for long-term mineral resource extraction purposes. It will be buffered from surrounding land uses on the north by Planning Area H, by the above-grade Southern Pacific Railroad embankment on the west, and by planned light industrial development in Planning Area K to the east.

General (non-aggregate truck) access to Cajon Boulevard from Area L should be through the southern portion of Planning Area K. Mineral resource extracted from this area will be transported directly to nearby Planning Area N for processing. A conveyor tunnel and/or vehicular undercrossing may be constructed under the railroad, provided proper authorization is secured, to allow movement of aggregate material from Planning Area L to Planning Area N for processing.

Should extractive activities commence in Planning Area L prior to light industrial development occurring in adjacent Planning Area K, a combination of earthen berms, landscaping and/or setbacks may be used as an "interim" buffer. An approved Conditional Use Permit and Reclamation Plan will be required. Planning Areas L, M and N may be combined within one Conditional Use Permit.

In concurrence with, or upon completion of extractive activities, portions of the extraction area may be re-filled with excess "fine" materials from the Planning Area N aggregate processing operation, or with suitable imported inert fill material. Over the long term, portions of Planning Area L, as with other on-site areas planned for mineral resource extraction, would be expected to be re-used for heavy industrial development, either by backfilling portions to grade, or at some sub-grade elevation.

Mining within Planning Area M could precede resource extraction in Planning Area L. Therefore, portions of Planning Area L, both prior to, as mining progresses, and after reclamation, may be used for open storage-related uses such as construction materials users activities, and other compatible uses such as contractor storage yards and automotive fleet storage.

Planned land uses would consist of the following:

Permitted Uses

Automotive and Equipment: Fleet Storage
Contractor Storage Yards
Construction Materials Users Activities
Mineral Resource Extraction and Processing and Related Uses (with Conditional Use Permit and Reclamation Plan)

Restricted Uses

Blasting associated with on-going mineral resource extraction activity shall not be permitted.

Retail Sales to consumers of Construction Material Users Park products produced, processed, packaged or warehoused on the premises are permitted.

Construction material users activities, contractor storage yards, and automotive fleet storage are permitted, but shall utilize facilities which are portable in nature or are readily removable upon completion of their use of the site. Such uses shall be compatible with adjacent on-site aggregate-related operations and truck traffic. Automotive wrecking yards shall not be permitted.

Design Guidelines

Development of Planning Area L as a mineral resource extraction area shall be in conformance with both CalMat Cajon Creek Specific Plan Design Guidelines, as well as the requirements of a Conditional Use Permit and Reclamation Plan approved by the City of San Bernardino. Both the CUP and the Reclamation Plan should encompass both Planning Areas L, M and N as all are directly related uses. Other permitted uses shall be in conformance with Specific Plan Design Guidelines.

Mineral resource extracted within Planning Area L shall be transported to and processed in Planning Area N. However, primary processing may occur in the active extraction area within Planning Area L. Such activities shall be limited to primary crushing

and/or removal of excess fines (scalping) prior to transport to the Planning Area N plantsite. Re-crushing of imported portland cement concrete and asphaltic concrete is also permitted.

Mining will be of the open quarry type, in accordance with an approved Mining and Reclamation Plan. Extraction may occur to a depth of about 120 feet. Fifty-foot setbacks shall be observed on all sides of Planning Area L, except where adjacent to the San Gabriel Valley Municipal Water District aqueduct, and along the east side of the railroad embankment which contains the CalNev petroleum products pipeline, where a 100-foot setback will be maintained. Mining and reclamation shall be conducted in phases. Mining should first occur along the southerly portion of Planning Area L and progress, in phases, from the easterly to the westerly portions of the site. All surface runoff from within the active extraction area shall be internally draining. An adequate supply of dust control water shall be provided.

Planned light industrial development within adjacent Planning Area K should be used to "buffer" extractive operations within Planning Area L. Should commencement of extractive operations precede such buffer industrial development, earthen berms and/or landscape screen vegetation shall be used to accomplish the same objective. A suitable buffer, in keeping with Specific Plan Design Guidelines, shall be provided to buffer extractive operations from the south. Planning Area L active extractive operations shall be enclosed with a six-foot chain link fence for safety reasons. This fence shall be utilized in conjunction with landscape screen vegetation, where appropriate, in keeping with Specific Plan Design Guidelines.

Upon completion of extraction operations and reclamation of Planning Area L, on-going fill operations may continue to occur within the extraction area using suitable imported inert fill. Such filling shall be in accordance with the City of San Bernardino Grading Ordinance, to result in a site which is suitable for building purposes, as provided for herein.

Upon completion of extractive operations within Planning Area L, and commencement of such operations in Planning Area M, Construction Material Users Park activities and other allowable uses in Planning Area M may be transferred to Planning Area L.

In the event that Planning Area M is mined prior to resource extraction occurring in Planning Area L, Planning Area L could be used as a Construction Material Users Park over the short term, or with similar uses to those planned for Planning Area M. After mining and reclamation of Planning Area M is completed, Construction Material Users Park, and other uses, could be relocated to Planning Area M, to allow mining in Planning Area L.

Zoning

(IE) Industrial Extractive

Development Standards

Gross Lot Area (minimum)	N/A
Floor Area Ratio (maximum)	N/A
Front Setback (minimum)	N/A
Rear Setback (minimum)	N/A
Side Setback - each (minimum)	N/A
Side Setback - street side (minimum)	N/A
Lot Coverage (maximum)	N/A
Structure Height (maximum)	N/A
Parking and Loading Standards (minimum)	N/A
Landscape Setback	N/A

PLANNING AREA M

Size: 97.5 Acres

Planned Use: Mineral Resource Extraction/Construction Material Users Park/Ultimate Heavy Industrial

Description. Planning Area M is an approximately 97.5-acre area bounded by the Metropolitan Water District and San Gabriel Valley Municipal Water District aqueduct on the north, the Cajon Creek 100-year floodplain and Muscoy Groin No.2 on the west, Planning Area N on the south, and the Southern Pacific Railroad and adjacent Planning Area L on the east.

Land Use Guidelines. Planning Area M is intended to be used for long-term mineral resource extraction. Rock, sand and gravel resources removed from this area will be processed in adjacent Planning Area N. Excess processing "fines", material not usable for marketable products, may be placed back into previously-extracted portions of Planning Areas L or M, as part of their reclamation plans. In addition, inert fill material could be imported into Planning Area M to backfill portions of the extracted area.

Area M may be extracted at some point well into the future, after Planning Area L is depleted of its resource. Should this occur, Planning Area M could be used as a Construction Materials Users Park in the interim, with uses similar to those planned for Planning Area L. After extraction and reclamation of Planning Area L is completed, it is expected that the Construction Material Users Park, and other allowable uses, would be relocated to Planning Area L, to allow extraction in Planning Area M. Should Planning Area M be extracted first, the opposite would occur.

Over the long term, portions of Planning Area M, as with other on-site areas proposed for mineral resource extraction, would be expected to be re-used for heavy industrial development or open storage-type uses by backfilling to grade or at some sub-grade location. Access to Planning Area M should be by means of an on-site access road along the western portion of Planning Area J to reach Institution Road. The potential for Construction Material Users Park, or other permitted uses, to access Cajon Boulevard by means of an access road across the southern portions of Planning Areas K and L also exists.

A conveyor tunnel and/or vehicular undercrossing may be constructed under the railroad to allow movement of aggregate material from Planning Area L across Planning Area M to Planning Area N for processing. Portions of the southern part of Planning

Area M may be used as an extension of the aggregate processing plantsite to be located in Planning Area N.

Planned land uses would consist of the following:

Permitted Uses

Automotive and Equipment: Fleet Storage
Contractor Storage Yards
Construction Material Users Activities
Mineral Resource Extraction and Processing and Related Uses (with Conditional Use Permit and Reclamation Plan)

Restricted Uses

Blasting associated with on-going mineral resource extraction activity shall not be permitted.

Retail Sales to consumers of Construction Materials Users Park products produced, processed, packaged or warehoused on the premises are permitted.

Construction material users activities, contractor storage yards, and automotive fleet storage are permitted, but shall utilize facilities which are portable in nature or are readily removable upon completion of their use of the site. Such uses shall be compatible with adjacent on-site aggregate-related operations and truck traffic. Automotive wrecking yards shall not be permitted.

Design Guidelines

Development of Planning Area M as a mineral resource extraction area with interim uses shall be in conformance with CalMat Cajon Creek Specific Plan Design Guidelines, as well as the requirements of a Conditional Use Permit and Reclamation Plan approved by the City of San Bernardino. Both the CUP and the Reclamation Plan should encompass Planning Areas L, M and N, as all are directly related uses.

Mineral resource extracted within Planning Area M shall be transported to and processed on adjacent Planning Area N. Primary processing may occur in the active extraction area within Planning Area M. Such activities shall be limited to primary crushing and/or removal of excess fines (scalping) prior to transport to the Planning Area N plantsite, although a portion of Planning Area N plantsite activities may occur on the southerly portion of Planning Area M, including the placement of settling ponds. Re-crushing of imported portland cement concrete and asphaltic concrete is also permitted.

Mining will be of the open quarry type, in accordance with an approved Mining and Reclamation Plan. Extraction may occur to a depth of about 120 feet. Fifty-foot setbacks shall be observed on all sides of Planning Area L, except where adjacent to the San Gabriel Valley Municipal Water District aqueduct, where a 100-foot setback shall be observed. Peripheral slopes may be mined to a 1:1 slope and backfilled and reclaimed to a 2:1 slope ratio, in accordance with an approved Reclamation Plan.

Mining and reclamation shall be conducted in phases. Mining will first occur along the southerly portion of Planning Area M and progress, in phases, from the westerly to the easterly portions of the site. All surface runoff from the active extraction area shall be internally draining. An adequate supply of dust control water shall be provided.

The permanent aggregate processing plantsite, to be located in Planning Area N immediately to the south, is expected to adequately buffer Planning Area M interim and extractive uses from the south. Such uses will be similarly buffered by the above-grade Southern Pacific Railroad on the east, Planning Area J on the north, and Muscoy Groin No. 2 along most of the west. The active Planning Area M extractive operation shall be enclosed with a six-foot chain link fence for safety reasons. This fence should be utilized in conjunction with landscape screen vegetation, where appropriate, in keeping with the Specific Plan Design Guidelines.

Upon completion of extractive operations and reclamation of Planning Area M, on-going fill operations may continue to occur within the extraction area, using suitable imported inert fill. Such filling shall be done in accordance with the City of San Bernardino Grading Ordinance, to result in a site which is suitable for building purposes, as provided for herein.

Prior to commencement of long-term extractive activities in Planning Area M, this area may be used as a Construction Materials Users Park and for other allowable uses. The re-crushing of imported portland cement concrete and asphaltic concrete shall also be permitted. Such uses shall be compatible with on-going mineral resource extraction and processing activities and related uses.

Interim utilization of Planning Area M shall be in conformance with CalMat Cajon Creek Specific Plan Design Guidelines. All interim use facilities shall be of a portable nature, or readily removable upon completion of such activities.

Appropriate landscape screening or other visual buffering, in keeping with Specific Plan Design Guidelines, shall be used where

necessary in order to attenuate views from the surrounding area. Adequate access shall be provided for both on-site uses, as well as the provision of an on-site access road for aggregate material transport trucks, connecting Planning Area J and Institution Road with Planning Area N to the south. If necessary, portions of Planning Area M may be used for providing rail access to Planning Area N.

Open storage and work areas related to interim uses shall be permitted. Such areas shall be screened from view from any non-similar adjoining properties, as determined necessary through Development Plan review, in keeping with Specific Plan Design Guidelines. Chain link fencing may be used without a landscaped buffer, where appropriate, provided it is not visible from outside Planning Areas M or N.

Zoning

(IE) Industrial Extractive

Development Standards

Gross Lot Area (minimum)	N/A
Floor Area Ratio (maximum)	N/A
Front Setback (minimum)	N/A
Rear Setback (minimum)	N/A
Side Setback - each (minimum)	N/A
Side Setback - street side (minimum)	N/A
Lot Coverage (maximum)	N/A
Structure Height (maximum)	N/A
Parking and Loading Standards (minimum)	N/A
Landscape Setback	N/A

PLANNING AREA N

Size: 70.0 Acres

Planned Use: Aggregate Processing Plantsite

Description. Planning Area N is an approximately 70-acre area located in the extreme southwestern portion of the Specific Plan area. It is a somewhat isolated triangular area located between the above-grade Southern Pacific Railroad and Cajon Creek. It is bounded on the north by Planning Area M. Approximately two-thirds of this area is presently within the Cajon Creek 100-year FEMA floodplain.

Land Use Guidelines. Planning Area N is to be used as a permanent rock, sand and gravel aggregate processing plant over the long term. Although no mining, other than material removed as part of plantsite preparation will occur, the aggregate plant will process the mineral resource extracted from Planning Areas L and M, and possibly Planning Areas F and P, although other alternatives exist to process the latter. The aggregate plant, which will provide for a full range of wet and dry processing, may also include a ready-mixed concrete plant, an asphaltic concrete plant, cement-treated base plant, portland cement concrete and asphaltic concrete re-crush operations, and other similar and related facilities. Rail shipment facilities may also be constructed. Primary access to the plantsite should be through the westerly portion of Planning Areas J and M to reach Institution Road. Alternative access could be available through the southerly portions of Planning Areas K, L and M to reach Cajon Boulevard, although it would be necessary to cross the Southern Pacific Railroad tracks. The southerly two-thirds of Planning Area N is currently within the FEMA-mapped Cajon Creek 100-year floodplain. However, a Hydrologic study (Appendix F) has indicated that most of Planning Area N (as shown in Figure 9) is not within the actual 100-year floodplain. No structural plantsite development activity, except for placement of aggregate material stockpiles or haul roads, shall occur within the presently-mapped FEMA 100-year floodplain within Planning Area N until the FEMA floodplain map has been revised.

Planned land uses would consist of the following:

Permitted Uses

Mineral Resource Extraction and Processing and Related Uses
(with Conditional Use Permit and Reclamation Plan)

Restricted Uses

Blasting associated with plantsite preparation activity shall not be permitted.

Design Guidelines

Development of Planning Area N as a mineral resource processing area shall be in conformance with the CalMat Cajon Creek Specific Plan Design Guidelines, as appropriate, as well as the requirements of a Conditional Use Permit approved by the City of San Bernardino for allowable mineral resource processing and related activities. The Conditional Use Permit should encompass related mining activities within Planning Areas L, M and N, as all are directly related uses.

Aggregate processing plant structures and related facilities may be permanently constructed. It is encouraged that, wherever feasible, they be low profile in nature. All permanent structures shall be located outside of the Cajon Creek 100-year FEMA floodplain. Plantsite layout and design should strive to locate operations which are noisiest and most prominent in nature in such a manner as to minimize impacts to scattered rural residences located to the southeast (and behind the Southern Pacific Railroad embankment) of Planning Area N. Active portions of the plantsite should be fenced with a six-foot chain link fence for safety purposes. This fence should, if appropriate, be used in conjunction with landscape screen vegetation, in accordance with Specific Plan Design Guidelines.

Landscaped earthen berms or other visual attenuation, such as the use of tall, fast-growing trees in combination with low-growing shrubs, should be used to break up the outline of plantsite operations from more distant views from the surrounding area. Species selected should be chosen for their drought tolerant capabilities, however, they must not present problems relating to leaves or other organic matter contaminating specification material stockpiles.

Should additional usable area be required, consideration should be given to locating portions of the plantsite within the southern part of Planning Area M, as this is planned to be an aggregate extraction area. Water utilized in wet aggregate processing operations shall be clarified and recirculated. An adequate water supply shall be provided.

Alternatively, should the limited portion of Planning Area N outside the Cajon Creek 100-year FEMA floodplain prove to not

have sufficient area to accommodate all facets of a permanent plantsite, it may be possible to expand some operational facets into areas within the presently-mapped FEMA 100-year floodplain, prior to official revision of the FEMA floodplain map. This may require Regional Water Quality Control Board and U.S. Army Corps of Engineers approval.

Adequate access should be provided across Planning Areas J and M in order to allow aggregate truck traffic from the plantsite to reach Institution Road, and then access the I-215 freeway by means of the Palm Avenue freeway interchange. This is the preferred route of aggregate truck travel, but it would require authorization to construct an access road across the Metropolitan Water District and San Gabriel Valley Municipal Water District aqueducts from those agencies. Alternatively, access could be provided across Planning Areas K, L and M to reach Cajon Boulevard, providing that an adequate undercrossing beneath the Southern Pacific Railroad between Planning Areas L and M can be achieved. Should the latter alternative be selected, the use of Cajon Boulevard south of Cable Creek Channel by aggregate plant truck traffic should be discouraged, except for local deliveries.

If determined to be feasible, rail shipment of aggregate in processed or partially processed form should be encouraged. Rail access may be developed in conjunction with Planning Areas M and J, in order to provide sufficient spur track length. If necessary, portions of the southern aspect of Planning Area N may be utilized in order to construct rail access, providing such areas are removed from the 100-year FEMA floodplain.

The Conditional Use Permit covering plantsite operations shall provide for adequate surface drainage control so that excessive erosion or sedimentation problems do not develop adjacent Cajon Creek. Proper containment of substances used on site, such as fuels, oils and lubricants, considered by State law to be hazardous, shall be provided. Prior to plantsite construction, all necessary permits shall be secured from the South Coast Air Quality Management District in order to assure that significant dust or air quality problems do not result.

Zoning

IE (Industrial Extractive)

Development Standards

Gross Lot Area (minimum)	N/A
Floor Area Ratio (maximum)	N/A
Front Setback (minimum)	N/A
Rear Setback (minimum)	N/A
Side Setback - each (minimum)	N/A
Side Setback - street side (minimum)	N/A
Lot Coverage (maximum)	N/A
Structure Height (maximum)	N/A
Parking and Loading Standards (minimum)	N/A
Landscape Setback	N/A

PLANNING AREA O

Size: 488.0 Acres

Planned Use: Open Space

Description: Planning Area O consists of an approximately 488-acre total area located within the 100-year floodplain of Cajon Creek. It extends southward from just south of Devore Road to just northwest of the former County landfill, and includes all of the CalMat ownership in that area westward of the Southern Pacific Railroad, except for that of Planning Area P. Planning Area O also includes approximately 180 acres of land owned by the County of San Bernardino Department of Flood Control which extends for a distance of approximately 3000 feet, to just north of Institution Road. Planning Area O continues southward within the Cajon Creek 100-year FEMA floodplain for a distance of approximately 0.6 mile to the south of Institution Road, encompassing all CalMat ownership within the Cajon Creek 100-year floodplain to the west of the Southern California Edison power line easement.

Land Use Guidelines: Planning Area O is intended to be used as open space. It may also be used for preservation and/or enhancement of biological habitats found in this area. It is planned that this area encompass portions of the CalMat Cajon Creek Specific Plan area which are not proposed for industrial development, mineral resource extraction or processing activities, or in-stream mining streambed stabilization purposes. Access to Planning Area P, across portions of Planning Area O will be permitted, but this should be done in a direct and/or environmentally-sensitive manner. All other portions of Planning Area O should be conserved for biological and open space purposes.

Planned land uses would consist of the following:

Permitted Uses

Open Space
Access to Planning Area P

Restricted Uses

No permanent structures shall be permitted.

Design Guidelines

All activities within Planning Area O shall be in conformance with the CalMat Cajon Creek Specific Plan.

Minimum disturbance of vegetation should occur within this area. Grading should be limited to that required to provide adequate haul road access to allow Planning Area P in-stream mining

activities, provide necessary flood control or to provide for necessary streambed stabilization purposes, as provided for within this Specific Plan. No activities should be permitted which would be incompatible with, create unnecessary safety or liability concerns, or preclude mining in Planning Area P. No permanent structures, other than flood control, streambed stabilization structures or haul roads associated with Planning Area P, shall be permitted within the 100-year floodplain. Easement rights, as they exist now or may be subsequently relocated, such as those associated with aqueducts, pipelines or powerlines, shall be maintained.

Maintenance or other activities associated with the provision of adequate flow capacity for a 100-year flood shall be permitted. Suitable areas may be set aside for biological enhancement purposes. If warranted in the future, after mining has been completed, portions of Planning Area O may be utilized for public or quasi-public purposes, such as for hiking and/or equestrian trails, or public utility purposes, provided that the property owner is fairly compensated for such use, excessive liability does not result, and operational safety of aggregate mining and processing operations, or industrial land use activities is not impaired. Illegal dumping and inappropriate off-road vehicle use within Planning Area O should be discouraged.

Zoning

PFC (Public Flood Control)

Development Standards

Gross Lot Area (minimum)	N/A
Floor Area Ratio (maximum)	N/A
Front Setback (minimum)	N/A
Rear Setback (minimum)	N/A
Side Setback - each (minimum)	N/A
Side Setback - street side (minimum)	N/A
Lot Coverage (maximum)	N/A
Structure Height (maximum)	N/A
Parking and Loading Standards (minimum)	N/A
Landscape Setback	N/A

PLANNING AREA P

Size: 257.0 Acres

Planned Use: Mineral Resource Extraction

Description. Planning Area P is an approximately 257-acre area located within the Cajon Creek 100-year floodplain. It extends from about 300 feet upstream of Institution Road to around one and three-quarter miles upstream of Institution Road. Approximately 100 acres within the southern aspect of Planning Area P will be leased from the County of San Bernardino.

Land Use Guidelines. Planning Area P is intended to be used for mineral resource extraction. It is planned that relatively shallow mining (skimming) will be conducted in order to remove material in excess of that which would be found in a "normal" channel configuration, that is, above the streambed's natural profile of equilibrium. Deep mining, which would create pits or areas of open water, is not permitted. The mining depth of the downstream end of the extraction area should be tapered to allow flood flows to maintain their pre-extraction velocities. The upstream end of the extraction area should be appropriately daylighted.

It is expected that infrequent major flood flows would tend to replenish the supply of aggregate material in this area available for extraction. No aggregate processing, other than possible primary crushing of material larger than six-inch diameter, and removal of excess fine material ("scalping") will be conducted in Planning Area P.

Planned land uses would consist of the following:

Permitted Uses

Mineral Resource Extraction

Restricted Uses

Blasting associated with on-going mineral resource extraction shall not be permitted.

Design Guidelines

Development of Planning Area P as a mineral resource extraction area shall be in conformance with the requirements of a Conditional Use Permit and Reclamation Plan approved by the City of San Bernardino.

Mineral resource extraction within Planning Area P shall be in conformance with the results of a "redline" engineering study performed by a qualified hydrologic engineer. Such a study should evaluate the "normal" channel profile (profile of equilibrium) of the Planning Area P reach of Cajon Creek and identify excess in-stream material that presently exists which can be removed without causing significant upstream or downstream effects, particularly on Institution Road, the eight-inch petroleum line located to the west, the Southern Pacific Railroad, or other adjacent areas of concern. This study shall also address stabilization of the upstream end, and measures to be taken at the downstream end to assure that floodflows maintain their pre-extraction velocities, so that excessive downstream sedimentation does not occur.

No aggregate processing, other than primary crushing of material larger than six-inches in diameter, or removal of excess "fine" material (scalping) with portable equipment, shall be conducted within Planning Area P because of its location within the Cajon Creek 100-year FEMA floodplain.

Aggregate extracted from Planning Area P could be processed at one of several locations. Such material could be conveyed or trucked beneath the Southern Pacific Railroad tracks eastward for processing at a portable plant to be located in Planning Area D, providing necessary authorization could be secured from the railroad. Alternatively, it could be processed at a portable plant which could be located in Planning Area I, or the material could be transported by truck to a main processing plant in Planning Area N. Transport of material extracted within Planning Area P will require traversing portions of Planning Area O open space. This shall be done in the most direct and/or environmentally sensitive manner.

No permanent structures shall be placed within the 100-year floodplain of Cajon Creek. No operating equipment, other than a portable primary crusher and/or scalping plant shall remain within Planning Area P during non-operating hours, or at any time surface flow is anticipated.

Mining should occur starting at the downstream end and progress in an upstream direction. No more area shall be exposed than can be extracted in a reasonable period of time. An approved mining and reclamation plan shall be followed, which provides for progressive recontouring and revegetation, as mining progresses.

Once initial extraction of areas within Planning Area P has occurred, replenished in-stream material resulting from flooding or stream flow in this area may again be extracted, in accordance with an approved Conditional Use Permit and Reclamation Plan.

Zoning

IE (Industrial Extractive)

Development Standards

Gross Lot Area (minimum)	N/A
Floor Area Ratio (maximum)	N/A
Front Setback (minimum)	N/A
Rear Setback (minimum)	N/A
Side Setback - each (minimum)	N/A
Side Setback - street side (minimum)	N/A
Lot Coverage (maximum)	N/A
Structure Height (maximum)	N/A
Parking and Loading Standards (minimum)	N/A
Landscape Setback	N/A

DESIGN GUIDELINES

VI. DESIGN GUIDELINES

A. Purpose

The following design guidelines are intended as a reference framework to be used by the designer in achieving high quality industrial development within the CalMat Cajon Creek Specific Plan area. Adapted from Section G19.08.050, Industrial Development Design Guidelines of the City of San Bernardino Development Code (1990), they provide good examples of potential design solutions. These guidelines complement the mandatory site development regulations contained within the City's Development Code, as well as those contained within the Specific Plan Planning Area Regulations. In addition to these Guidelines, each Planning Area in the Specific Plan has Design Guidelines that also apply to development in that Planning Area.

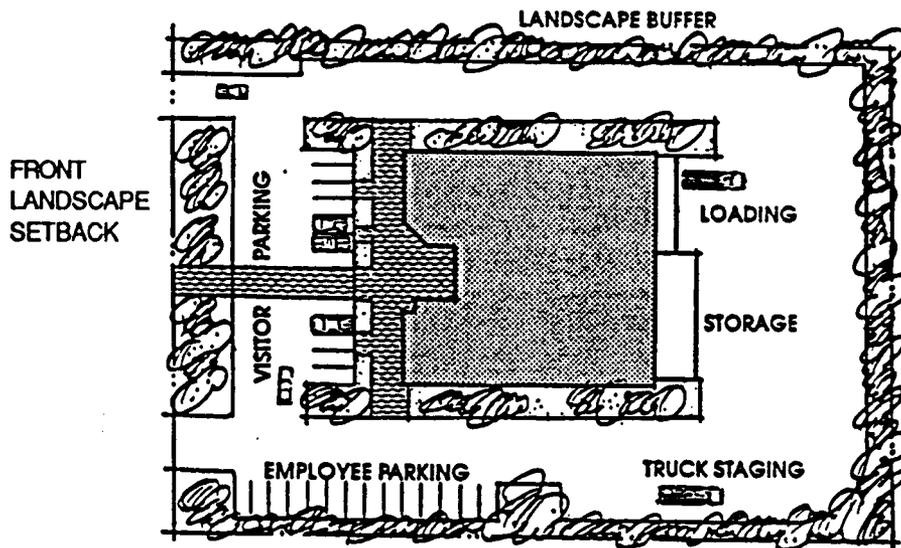
These design guidelines are general and may be interpreted with some flexibility in their application in order to encourage design creativity. They will be utilized as design criteria during the review process which will be conducted by the City in approving individual Development Permits.

The provisions of this section shall apply to all industrial development within the CalMat Cajon Creek Specific Plan area. Any additional remodeling, relocation or construction requiring a building permit and subject to review by the City shall adhere to these guidelines, where applicable.

B. Site Planning Principles

1. The main elements of sound industrial site design, as set forth in the City's Development Code, include the following principles:
 - Controlled site access.
 - Service areas located at the sides and rear of buildings.
 - Convenient access, visitor parking and on-site circulation.

- Screening of outdoor storage, work areas and equipment.
 - Emphasis on the main building entry and landscaping.
 - Landscaped open space.
2. A variety of buildings and parking setbacks should be provided in order to avoid long, monotonous building facades, and to create diversity.
 3. Structures should be located on "turf islands", where the office portion of the building does not directly abut paved parking areas.



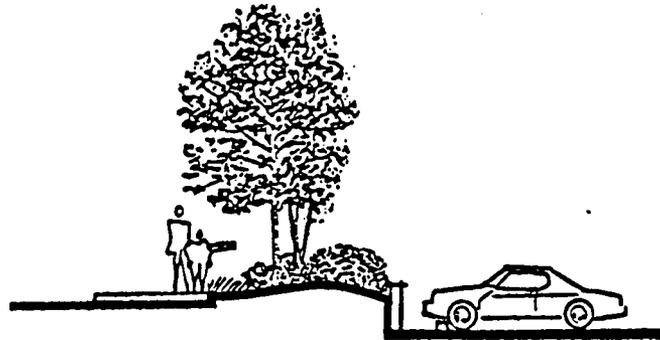
4. Building setbacks should be provided proportionate to the scale of the structure and in consideration of existing development adjacent to it. Larger structures require more setback area for a balance of scale, so as not to impose on neighboring uses.
5. Building placement which creates opportunities for plazas, courts or gardens, is encouraged.

6. Where industrial uses are adjacent to non-industrial uses, appropriate buffering techniques such as setbacks, screening, and landscaping shall be provided to mitigate any negative effects of industrial operations.
7. Proposed development should be designed to preserve existing mature trees, wherever possible.
8. Building entries for visitors and employees should be clearly identified and of a human scale.

C. Parking and Circulation

1. The parking lot and cars should not be the dominant visual elements of the site. Large expansive paved areas are to be avoided in favor of smaller multiple lots separated by landscaping and buildings.
2. Site access and internal circulation should be designed in a straightforward manner which emphasizes safety and efficiency. The circulation system should be designed to reduce conflicts between vehicular and pedestrian traffic, combine circulation and access areas where possible, provide adequate maneuvering and stacking areas, and consideration for emergency vehicle access. Circulation routes and parking areas should be separated.
3. Entrance and exits to and from parking and loading facilities should be clearly marked, with appropriate directional signage where multiple access points are provided.
4. Vehicles should not be required to enter the street in order to move from one area to another on the same site.

5. Parking lots adjacent to and visible from public streets must be adequately screened from view through the use of rolling earthen berms, low screen walls, changes in elevation, landscaping or combinations thereof, whenever possible.

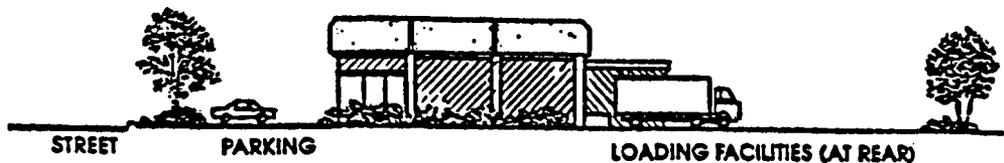


Parking areas should be screened from public view.

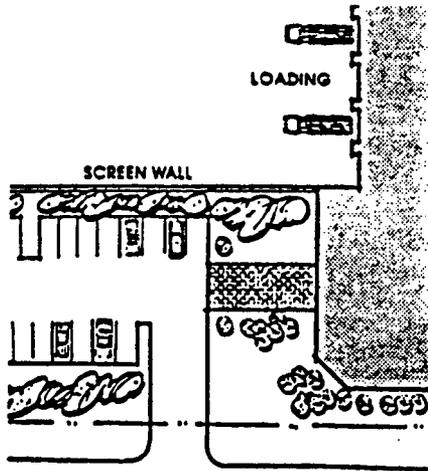
6. The industrial site should be a self-contained development capable of accommodating its own parking needs. The use of the public street for parking and staging of trucks is not allowed.
7. All parking spaces should be visible from structures, especially entrances.
8. Parking shall not be permitted within the front landscaped setback.
9. Wherever feasible, existing mature trees should be integrated into parking design.

D. **Loading Facilities**

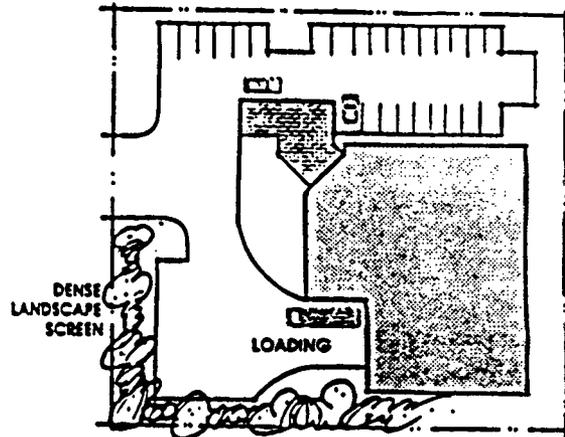
1. In order to alleviate the unsightly appearance of loading facilities for industrial uses, such areas should not be located at the front of buildings where it is difficult to adequately screen them from view.



2. When it is necessary to locate loading facilities along the side of the building, loading docks and doors should not be dominant, and must be screened from the street. Such loading facilities should be offset from driveway openings.
3. Backing from the public street onto the site for loading into front end docks causes unsafe truck maneuvering and shall not be permitted.



Use decorative solid masonry wall to screen loading areas.

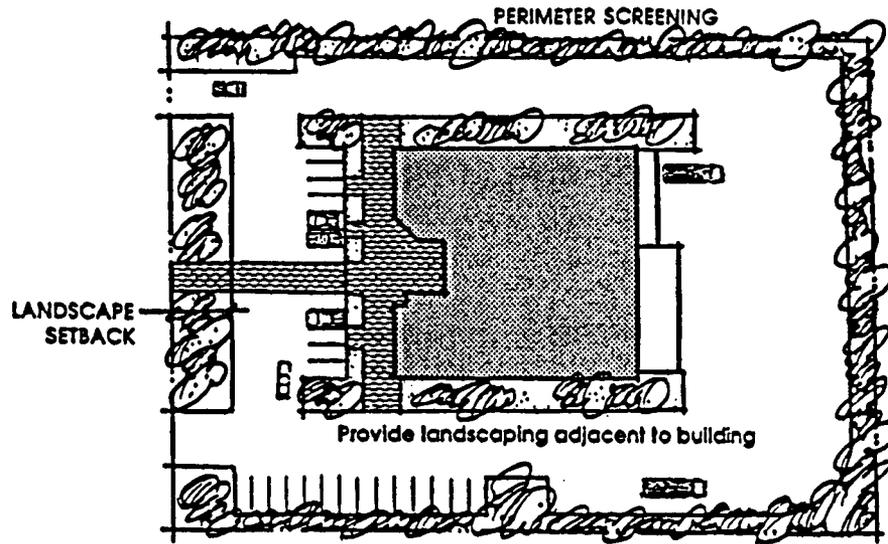


Offset the loading areas from the driveway openings.

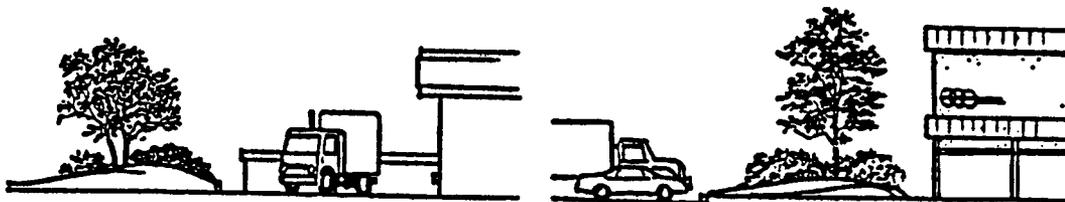
4. Rail loading facilities for industrial buildings and facilities should be guided by the above design guidelines, to the greatest extent feasible.

E. Landscaping

1. Landscaping and open spaces shall be designed as an integral part of the overall site plan design. They should accent and enhance building design, public views and spaces, provide buffers and transitions, provide for a balance of solar uses, and provide screening.
2. Landscaping for industrial uses should be used to define areas by helping to focus on entrances to buildings, parking lots, loading areas, defining the edges of various land uses, providing transition between neighboring properties (buffering), and providing screening for outdoor storage, loading, and equipment.



3. Landscaping should be in both scale and harmony with adjacent buildings, and be of appropriate size at maturity to accomplish its intended goals.
4. Planting areas between walls and streets should be landscaped in a hierarchy of plants in natural formations and groupings. The use of vines on walls in industrial areas is appropriate, because such walls often tend to be large and blank.
5. A landscaped edge should be established at the base of buildings. Landscaping around the entire base of buildings is recommended in order to soften the edge between the parking lot and the structure. It should be accented at entrances in order to provide focus. Plant materials located in containers are appropriate. The use of seasonal colors is encouraged.
6. Trees should be located throughout the parking lot and not simply at the ends of parking aisles.
7. Landscaping should be protected from vehicular and pedestrian encroachment by raised planting surfaces, depressed walks, or the use of curbs. Concrete mow-strips are required between turf and shrub areas.



8. Planting masses on-site should assure a simple, non-uniform arrangement. The diversity of massing types should be great enough to provide interest, but kept at a level which evokes a relaxed, natural feeling.
9. The following are common planting design concepts that should be used whenever possible:
 - Specimen trees used in informal grouping and rows at major focal points.
 - Pots, vases, wall or raised planters.
 - The use of planting to create shadow and patterns against walls.
 - Trees to create canopy and shade, especially in parking areas.
 - The use of flowering trees in informal groups to provide color.
 - Use of distinctive plants as focal points.
 - Berms, plantings, and low walls to screen parking areas from view of public rights-of-way, while allowing filter views of larger buildings beyond.
10. Landscape design and construction should emphasize drought-tolerant landscaping whenever possible.
11. All landscaping shall have an approved automatic irrigation system.
12. Trees should be long-lived, be clean, require little maintenance, be structurally strong, insect and disease resistant, and require little pruning.
13. Landscaping in areas adjacent to open space portions of the Specific Plan within the Cajon Creek 100-year floodplain shall be designed and managed so as to minimize wildfire risks.

14. Trees and shrubs shall be planted so that, at maturity, they do not interfere with service lines, traffic sight distance, and basic property rights of adjoining owners, particularly the right of solar access.
15. Landscaping should be installed and maintained in the following manner:
 - Plant material should be strong, healthy and vigorous. Trees, shrubs, and vines should have body and fullness that is typical of the species.
 - All ground cover should be healthy, densely foliated, and well-rooted cuttings, or one gallon container plants. Herbaceous ground covers should be planted no more than 12 inches on center, and woody shrubs should be planted no more than 3 feet on center.
 - There shall be an average of one tree for every 10 feet of street frontage within the front yard setback.
 - The spacing of trees and shrubs should be appropriate to the species used. The plant materials should be spaced so that they do not interfere with the adequate lighting of the premises or restrict access to emergency apparatus such as fire hydrants or fire alarm boxes. Proper spacing should also insure unobstructed access for vehicles and pedestrians, in addition to providing clear vision of the intersections from approaching vehicles. Plant material should conform to the following spacing standards:

A minimum of 25 feet from the property corner at a street intersection to the center of the first tree or large shrub.

A minimum of 15 feet between center of trees and large shrubs to light standards.

A minimum of 15 feet between center of trees or large shrubs and fire hydrants.

A minimum of 10 feet between center of trees or large shrubs and edge of driveway.

- Plant material shall conform to the following planting size or coverage:

Trees: 35%, 24-inch box
 50%, 15-gallon

In addition, mature trees shall be provided for variety and emphasis of focal areas as follows:

15% mature specimen trees in 36-inch box

Existing mature trees may be utilized in lieu of specimen tree planting

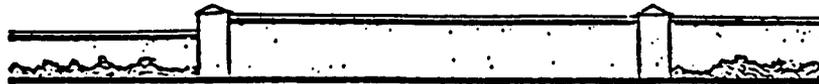
Shrubs: 50%, 5-gallon
 50%, 1-gallon

Groundcover: 100% coverage within one year

F. Walls and Fences

1. Walls can serve a major function in the industrial landscape and can be used to screen automobiles, loading and storage areas, and utility structures. Such walls should be kept as low as possible, while adequately performing their screening and security function.
2. Where walls are used at property frontages, or screen walls are used to conceal storage and equipment areas, they should be designed to blend with the site's architecture. Landscaping should be used in combination with such walls.
3. The use of chain link fencing is not permitted within setback areas adjacent to streets. When security fencing is required, it should be attractively designed.

4. Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets should be provided.



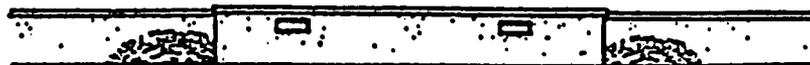
Elevation of Solid Wall with Pilasters



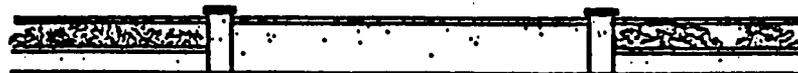
Elevation of Wall /Wrought Iron Combination



Elevation of Wrought Iron with Pilasters



Elevation of Staggered Wall

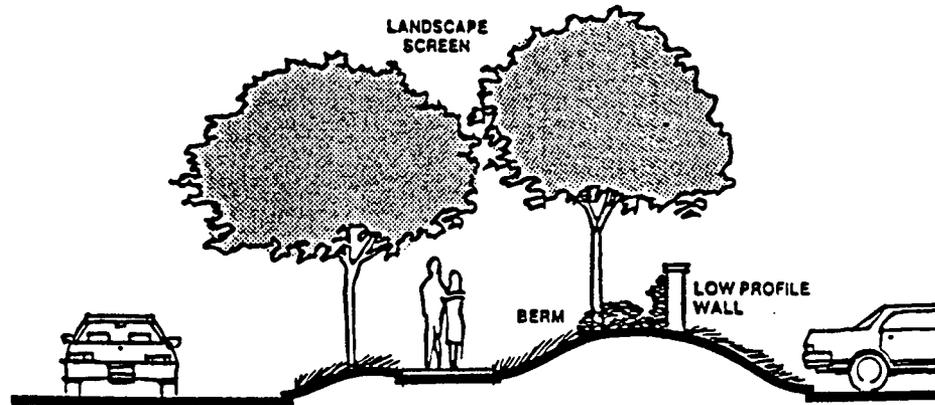


Elevation of Planters/Wall

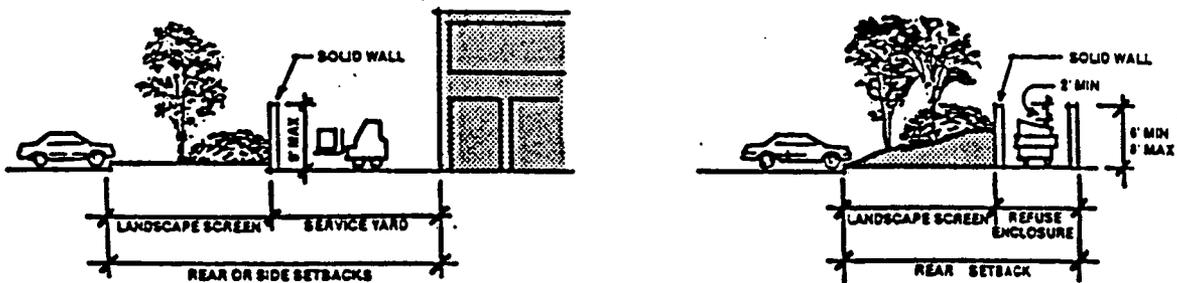
G. Screening

1. Screening for industrial outdoor storage should be a minimum of 8 feet and a maximum of 12 feet high. The height should be determined by the height of the material being screened. Exterior storage

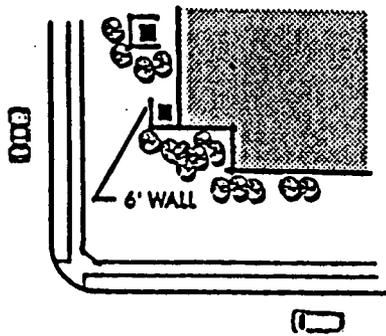
should be confined to portions of the site least visible to public view. Refuse storage shall be screened in accordance with Chapter 19.20 of the City of San Bernardino Development Code.



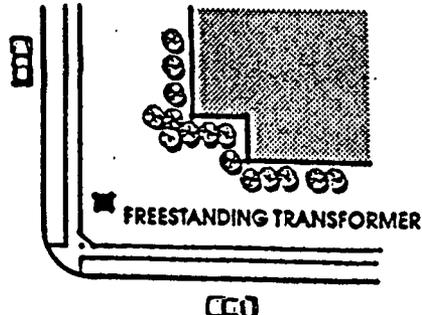
- Where screening is required, a combination of elements should be used, including solid masonry walls, berms, and landscaping. Chain link fencing with wood or metal slatting shall only be used in areas where the use of chain link fencing is specifically provided for in the Planning Area Regulations.



- Any equipment, whether on the roof, side of building, or ground, shall be screened. The method of screening shall be architecturally integrated in terms of materials, color, shape, and size. The screening design shall blend with the building design. Where individual equipment is provided, a continuous screen is desirable.



DO THIS



DONT DO THIS

4. The need to screen roof top equipment should be taken into consideration during the initial architectural design phase of the structure.
5. Landscape screening for industrial development purposes should be of an opaque type. Such a screen should, at maturity, be opaque from the ground to a height of at least six feet, with intermittent tall screening trees or shrubs from the opaque portion to a height of at least 20 feet. An opaque screen is intended to exclude all visual contact between users and create a strong impression of spatial separation. It must be opaque in all seasons of the year, and should not contain any completely unobstructed openings more than 10 feet wide.

TYPICAL OPAQUE SCREENS



SMALL TREES PLANTED
30' ON CENTER.
6' HIGH EVERGREEN
SCREENING SHRUBS
PLANTED 4' O.C.



LARGE TREES PLANTED
40' ON CENTER.
6' HIGH
MASONRY WALL



TALL EVERGREEN TREES
STAGGER PLANTED,
WITH BRANCHES
TOUCHING GROUND.

H. Architectural Design

1. As a category of building types, industrial buildings often present unattractive and monotonous facades. There are, however, a variety of design techniques available to help overcome this situation and to direct development into a cohesive design statement. These include:

- Employment of a variety in building forms, to create visual character and interest.
- Avoidance of long "unarticulated" building facades. Building facades with varied front setbacks are strongly encouraged.
- Avoidance of blank front and side wall elevation on street frontages.
- Entries to industrial buildings should portray a quality office appearance, while being architecturally tied into the overall building mass and building composition.
- Windows and doors are key elements of any building's form, and should relate to the scale of the elevation on which they appear. Windows and doors can establish character by their rhythm and variety. Recessed openings help to provide depth and contrast on elevation planes.
- Sensitive alteration of colors and materials can produce diversity and enhance architectural forms.
- The staggering of planes along an exterior wall elevation creates pockets of light and shadow, providing relief from monotonous, uninterrupted expanses of wall.

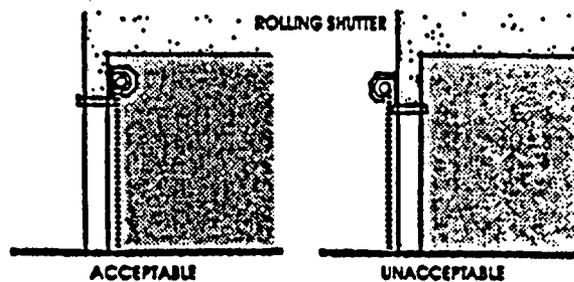
2. Design elements which are undesirable and should be avoided include:

- Highly reflective surfaces whose orientation would create undesirable light patterns.
- Exposed, untreated precision block walls.

- Chain link fence, barbed wire (except as specifically provided for in the Planning Area Regulations).
 - False fronts.
 - "Stuck on" mansard roofs on small portions of the roofline.
 - Unarticulated building facades.
 - Materials with high maintenance, such as stained wood, shingles or metal siding.
3. Choose wall materials that will withstand abuse by vandals or accidental damage from machinery.
 4. No permanent metal buildings will be permitted.
 5. Berming in conjunction with landscaping can be used at the building edge to reduce building mass and height along facades.



6. Rolling shutter doors located on the inside of the building is preferred method for providing large loading doors, while maintaining a clean, uncluttered appearance from the exterior.



7. Passive design concepts, which make use of the natural climate to increase energy efficiency, should be utilized, in accordance with State law.
8. Because of the Specific Plan area's location within a High Wind Hazard Area, all buildings shall be constructed and sited to withstand high wind hazards, and to prevent adverse funneling of wind on-site and on adjacent properties. Industrial buildings shall be designed to prevent wind funnel effects around their base and in passage-ways.

I. Roofs

1. The roofline at the top of the structure, whenever possible, should not run in a continuous plane.
2. Nearly vertical roofs (A-frames) and mansard roofs are prohibited.
3. All roof equipment must be screened from public view by screening materials of the same nature as the building's basic materials. Mechanical equipment should be located below the highest vertical element of the building.
4. The following roof materials are not acceptable:
 - Corrugated metal (standing rib metal roofs are permitted).
 - High contrast of brightly colored glazed tile.
 - Highly reflective surfaces.
 - Illuminated roofing.

J. Signage

1. Every structure should be designed with a precise concept for adequate signing. Provisions for sign placement, sign scale in relationship with building and the readability of the sign should be considered in developing the overall signing concept. All signs should be highly compatible

with the structure and site design relative to color, material and placement.

2. Monument-type signs are the preferred alternative for business identification.
3. The use of backlit individually-cut letter signs is strongly encouraged.
4. The industrial site should be appropriately signed to give directions to loading and receiving area, visitor parking and other special areas.
5. Box-type signs are not permitted on building facades.
6. Multi-tenant signs shall be limited to one-sixth of the Suite width and no greater than 24 inches in height.
7. All other provisions of Chapter 19.22, Sign Regulations, of the City of San Bernardino Development Code shall apply.

K. Lighting

1. Lighting should be used to provide illumination for the security and safety of on-site areas such as parking, loading, shipping and receiving, pathways, and working areas.
2. The design of light fixtures and their structural support shall be architecturally compatible with main buildings on-site. Illuminators should be integrated within the architectural design for the buildings.
3. As a security device, lighting should be adequate but not overly bright. All building entrances should be well lighted.
4. All lighting should be shielded to confine light spread within the site boundaries.
5. One footcandle average maintained across a parking lot, with a 15 to 1 maximum to minimum ratio, is the required minimum. At entrances and loading areas, up to two footcandles may be appropriate.

L. Mining

The following design guidelines are intended to assure that mining and aggregate processing operations are conducted in a manner in keeping with the development standards set forth by the CalMat Cajon Creek Specific Plan area. They are designed to complement site-specific design guidelines contained within the Planning Area Regulations. Together, they should be used as criteria in approving Conditional Use Permits and Reclamation Plans for mineral resource extraction or aggregate processing.

1. All mining (extraction) operations shall have a minimum 50-foot setback from Planning Area boundaries. Setback requirements for aggregate processing plants shall be determined as part of the Conditional Use Permit process on a site-specific basis.
2. Mining and aggregate processing operations shall be sited and screened so that surrounding uses are adequately buffered.
 - A combination of elements, such as setbacks, earthen berms and landscaping may be utilized.
 - Adjacent industrial development, as provided for by the Specific Plan, may be used to "buffer" mining or aggregate processing activities from surrounding areas. Should commencement of mining or processing operations precede such planned development, interim earthen berms and/or landscape screen vegetation should be used to accomplish the same objectives.
 - Landscaped earthen berms and/or fast-growing trees in combination with low-growing shrubs should be used to break up the outline of permanent aggregate processing plantsite operations from more distant view. Vegetative species should be selected for their drought-tolerant capabilities, however they must not present problems relating to leaves or other organic matter contaminating specification material stockpiles.

- Landscape screening should be of an opaque type. At maturity, such screening should be opaque from the ground to a height of at least six feet, with intermittent tall screening trees or shrubs from the opaque portion to a height of at least 20 feet. It must be opaque in all seasons of the year and should not contain any completely unobstructed openings more than 10 feet wide.
 - More prominent aspects of the aggregate processing operation should be sited away from adjacent land use, to the greatest extent feasible.
 - Where feasible, material stockpile areas should be used to attenuate noise from surrounding areas.
 - Aggregate processing facilities should be designed to maintain as low a profile as possible and, where appropriate, should be painted a color that does not contrast with the surrounding landscape.
 - Required security fencing around active mining or aggregate processing operations should be attractively designed. When possible, the security fence should be integrated into the design of the berm and landscape screen vegetation.
3. All mining and aggregate processing operations shall be conducted in accordance with an approved Conditional Use Permit and Reclamation Plan.
- Finished extraction area slopes should be stable and of a minimum 2 to 1 (horizontal to vertical) slope ratio. Active mining slopes may be of a 1 to 1 slope ratio.
 - Excess "fine" material from aggregate processing operations may be used to blend in with suitable imported inert fill material to refill portions of previously-extracted areas. Such refilling may occur over the long term, after completion of mining operations. The standards for compaction of the fill should be

appropriate for the post-mining uses, as provided for by the Specific Plan.

- Mining and reclamation should occur in a phased manner. No more vegetation should be removed in advance of extraction than is necessary, and mined areas should be progressively reclaimed.
 - Finished slopes should be revegetated with native plants, in accordance with an approved Reclamation Plan. Such slopes should be seeded with a mixture of native grasses, herbaceous material and shrubs, and where appropriate, trees.
 - Interim aggregate processing operations and related facilities should be of a portable nature, or readily removable upon completion of such activities.
 - No permanent aggregate processing structures or facilities, except for placement of material stockpiles or haul roads, shall be located within the FEMA-mapped 100-year floodplain.
4. An adequate supply of water for aggregate processing, dust control and landscape screen vegetation shall be provided. Water utilized by wet aggregate processing operations should be clarified and re-utilized.
5. Adequate mining area and aggregate processing plantsite drainage shall be provided.
- All surface runoff from within the extraction area should be internally draining, except in the Cajon Creek floodway, where such runoff shall be conveyed through or around the extraction area.
 - Aggregate processing plantsite surface drainage should be controlled so that erosion or sedimentation does not adversely affect Cajon Creek.
6. Adequate access to mining and aggregate processing areas shall be provided.

- The use of Cajon Boulevard south of the Cable Creek Channel by aggregate truck traffic should be discouraged, except for local deliveries.
 - Consideration should be given to the feasibility of future rail shipment of processed or unprocessed aggregate.
7. All necessary permits shall be secured from the South Coast Air Quality Management District in order to assure that significant dust or air quality problems do not result.
 8. Proper containment of substances used on-site, such as fuels, oils and lubricants, considered by State law to be hazardous, shall be provided.
 9. In-stream mining shall be conducted to minimize or prevent off-site erosion.
 - In-stream mining shall be conducted in a manner that maintains pre-extraction velocities so that excessive downstream sedimentation or upstream erosion does not occur.
 - Shallow mining ("skimming") may be conducted in order to remove material in excess of that which would be found in a "normal" channel configuration.
 - Deep mining, which would create pits or areas of open water should not be permitted.
 - No aggregate processing, other than primary crushing and/or removal of excess fines and placement of temporary stock-piles, should be permitted within the FEMA 100-year floodplain.
 - Haul roads across open space areas should be traversed in the most direct and/or environmentally-sensitive manner possible.
 - Replenished material within previously-extracted areas resulting from flood flow may be re-extracted.